

III. ELEMENTS OF THE SPECIFIC PLAN

A. LAND USE

The policies of the General Plan and the Framework Plan apply to Del Mar Mesa, with the exception of those that require a phase shift to implement. In addition, Council Policy 600-29 provides guidelines for development within the FUA. The General Plan goals for the FUA are to "avoid premature urbanization, to conserve open space and natural environmental features and to protect the fiscal resources of the City by precluding costly sprawl and/or leapfrog development." The overall goal for Del Mar Mesa is as follows:

GOAL: TO PRESERVE THE RURAL CHARACTER OF DEL MAR MESA WHILE ACCOMMODATING CLUSTERED DEVELOPMENT AND THE PRESERVATION OF OPEN SPACE

Approximately 685 dwelling units are permitted within Del Mar Mesa based on the Planned Development Permit (PDP) provision which allowed a maximum of one dwelling unit/four acres for the AR-1-1 areas and the AR-1-2 zone, which permits a density of one dwelling unit per acre. (See **Table 2**). The existing AR-1-1 zoning would have allowed a maximum of one dwelling unit per four acres under the rural cluster option of the Planned Residential Development permit. In order to preserve a large, contiguous open space area in the eastern part of the community, the density was planned for the western portion of the community at a rate of one dwelling unit per 2.5 acres. The areas zoned AR-1-1 in the eastern portion of the community have a maximum density of one dwelling unit per ten acres and no longer have the option to develop under the rural cluster alternative of one dwelling unit per four acres. The number of dwelling units that may be permitted in the community may be reduced as properties and their associated rights are purchased in the Resource Based Open Space area located in the eastern portion of the community. Additional units could be accommodated in the community using the Affordable Housing Density Bonus provision of the Municipal Code. The Del Mar Mesa Specific Plan proposes to concentrate this development on the western half of the Del Mar Mesa where use will be characterized by low-density residential development. Virtually the entire eastern half of Del Mar Mesa is intended for possible open space acquisition and accommodation of limited development consistent with the underlying zoning. **Figure 5** shows the overall development program for the Del Mar Mesa.

1. Development Area

The Del Mar Mesa Specific Plan defines the core development area in the western half of Del Mar Mesa. Within this area, substantial structural development as well as major and minor roads are expected to occur. Almost all of the development in the Del Mar Mesa will consist of very low-density residential development falling into the Estate Residential category. In addition to residential development, the Del Mar Mesa Specific Plan also designates a golf course and a resort hotel within the southwestern portion of the community.

**TABLE 1
LAND USE**

| LAND USE | ACREAGE |
|-------------------------------------|-------------------|
| Estate Residential (includes hotel) | 563.0 ac. |
| Open Space | 1270.4 ac. |
| Resource Based | 208.6 ac. |
| Urban Amenity | |
| TOTAL | 2042.0 ac. |

2. Residential Land Use

While the principal use will be large-lot single-family homes, other kinds of residential development (e.g., clustered single-family dwellings or companion units) will be allowed pursuant to a planned development permit. Due to the rural nature of the community and the desire to preserve natural and undisturbed open space rather than artificial, usable open space, the open space per dwelling unit required by the planned development permit shall not apply to Del Mar Mesa. A planned development permit may be used to achieve more units on a site designated as Estate Residential provided that the additional density is achieved by including density transferred from a site within Del Mar Mesa and designated as Resource Based Open Space. The site providing the additional density must be designated for Resource Based Open Space, would no longer have density associated with it and would be conserved by either easement or dedication to the City. The **Implementation** and **Community Design Element** sections in this Plan identify both general and site-specific development regulations for parcels within the Del Mar Mesa.

Table 3 outlines the dwelling unit allocation for Del Mar Mesa. This is summarized below:

- a. Parcels designated for Open Space/Rural Residential development are assigned a maximum of one dwelling unit per ten acres consistent with the underlying zoning and may be considered for open space acquisition.
- b. Areas zoned AR-1-2 designated for development would retain the current density and could develop to a maximum of one dwelling unit/acre.
- c. Parcels in City ownership are designated as Resource Based Open Space and are not proposed for any development. Parcels in federal, state and county ownership, and City enterprise fund departments that are zoned AR-1-1, are assigned a maximum of one dwelling unit/ten acres.
- a. Approximately 415 dwelling units are assigned to parcels zoned AR-1-1 that are all or partially designated as Estate Residential. This equates to a maximum of one dwelling unit/2.5 acres for the areas designated for Estate Residential. Approximately 190 units are assigned to parcels zoned AR-1-2 and designated for Estate Residential. The maximum density for both zones is calculated over the gross area of the parcel with development intended to be located on all or a portion of the site designated for development.

TABLE 2
DEL MAR MESA - LAND USE ACREAGES

| LAND USE AREAS | ACREAGE | | TOTAL |
|--|-------------------------|-----------|-----------------------|
| | AR-1-1 | AR-1-2 | |
| Private parcels designated for Estate Residential | 1041.9 ac. | 190.2 ac. | |
| Private parcels designated for Rural Residential and possible Open Space acquisition | 358.4 ac. | 30.1 ac. | |
| Publicly-owned open space | 531.4 ac. | 20.0 ac. | |
| Subtotal | 1931.7 ac. | 240.3 ac. | |
| Portions of ownerships that extend outside Del Mar Mesa | -130.0 ac. | | |
| TOTAL | 1801.7 ac. ³ | 240.3 ac. | 2042 ac. ³ |

Total Dwelling Units Potentially Permitted Without a Phase Shift

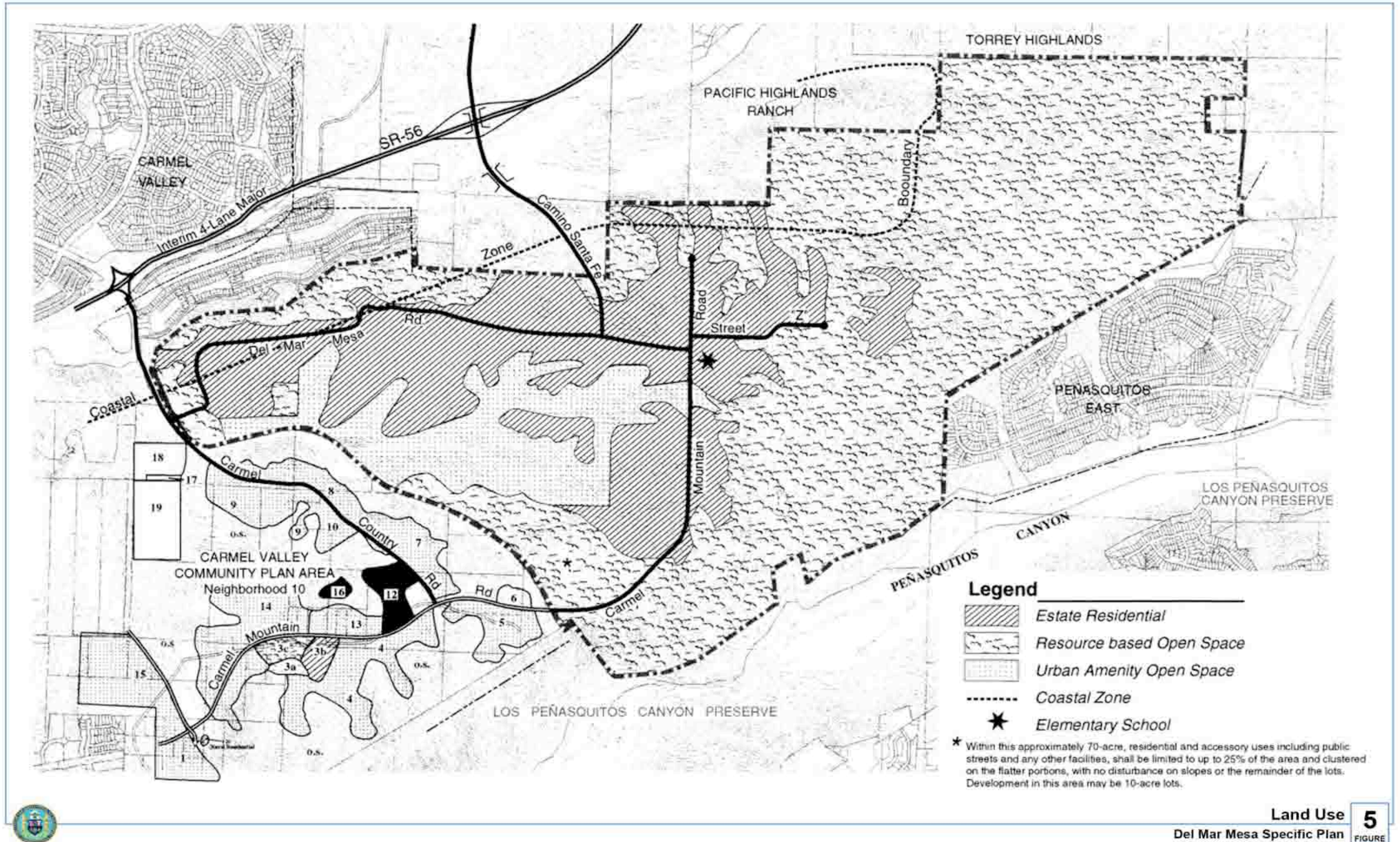
| | |
|----------------------|---|
| 1781.7 ac. @ 1du/4ac | = 445 du's ¹ |
| 240.3 ac. @ 1du/ac | = 240 du's |
| Total | = 685 dwelling units^{2,3} |

1. One 20-acre parcel was purchased for mitigation prior to August 1, 1984, the effective date of Proposition A. Development rights associated with this parcel are ineligible for reallocation, reducing the total AR-1-1 zoned acreage on which potential density reallocation is calculated from 1801.7 acres to 1781.7 acres.
2. Additional units could be achieved in Del Mar Mesa under the Affordable Housing Density Bonus provision of Municipal Code.
3. Since the adoption of the Del Mar Mesa Specific Plan, on July 30, 1996, 95.6 acres have been conserved either by purchase or easement for the purpose of conservation and mitigation. The units associated with this acreage will not be eligible for relocation, reducing the total AR-1-1 zoned acreage on which potential density allocation is calculated from 1781.7 acres to 1686.1 acres.

**TABLE 3
DWELLING UNIT ALLOCATION**

| LAND USE AREAS | AR-1-1 ZONE | | AR-1-2 ZONE | | TOTAL |
|--|-------------|--------------------------------------|-------------|-----------------|-----------------------------|
| | ACREAGE | DUs | ACREAGE | DUs | |
| Private parcels designated for Estate Residential | 1041.9 ac. | 415 du's (1du/2.5a ¹) | 190.2 ac. | 190 du's | 605 du's |
| Private parcels designated for Rural Residential and possible Open Space acquisition | 358.4 ac. | 35 du's | 30.1 ac. | 30 du's | 65 du's |
| City-owned | 412.8 ac. | 4 du's ² | 20.0 ac. | 0 du's | 4 du's |
| Publicly-owned - other jurisdictions | 118.7 ac. | 11 du's | 0 ac. | 0 du's | 11 du's |
| TOTAL | | 465 du's | | 220 du's | 685 du's³ |

1. Development rights equal to nine dwelling units are reallocated from ownership area 50 to area 70, contingent on Council approval of Carmel Valley Neighborhood 8A development agreement, or a similar agreement. Transfer of these units reduces the total number of dwelling units available for allocation to 406, yielding a density of one du per 2.5 gross acres in areas zoned AR-1-1 with development shown.
2. Density is allocated to City enterprise fund departments according to the existing AR-1-1 zoning. This includes one 17.5-acre parcel owned by the Environmental Services Department (APN 306-050-11) and two parcels totaling 30 acres owned by the Metropolitan Wastewater Department (APN 306-050-07, 21).
3. Since the adoption of the Plan on July 30, 1996, 95.6 acres have been conserved either by purchase or easement for the purposes of conservation and mitigation. The units associated with this acreage will not be eligible for relocation, reducing the number of the total AR-1-1 zoned acreage on which potential density allocation is calculated from 1781.7 acres to 1686.1 acres.



3. Affordable Housing

The City of San Diego's "Consolidated Plan" documents the City's need for affordable housing. The plan states that nearly 107,000 very-low and low-income households in San Diego would require assistance for their housing to be affordable. The lack of affordable housing is not only a social issue affecting communities but also has a negative impact on the local economy. Providing housing opportunities affordable to those working in low-wage jobs benefits the City as a whole.

To help address its need for affordable housing, the City encourages the provision of affordable housing opportunities throughout its many communities, in part, through Council Policy 600-19 concerning balanced communities and through the Future Urbanizing Area Affordable Housing Requirement contained in the City's PDP regulations and addressed further in the North City Future Urbanizing Area Framework Plan.

Although Del Mar Mesa is likely to include many small residential developments with residents relying on services located outside the area, development plans which primarily call for large homes on large lots suggest that household help may be desired to care for the house and grounds. Furthermore, the resort hotel and golf course both provide relatively low-wage employment opportunities. It is clear that affordable housing provided in Del Mar Mesa will help address the needs created in the community and contiguous areas.

Residential development in Del Mar Mesa must provide for affordable housing as required of all such development in the FUA. However, property owners are permitted to meet the affordable housing requirement off-site, if desired, due to the rural character and the small size of developments proposed for the Del Mar Mesa. The requirement specifies that residential development projects must provide housing affordable to low-income families as certified by the San Diego Housing Commission. This requirement can be fulfilled by the following:

- a. The provision of units through new construction or acquisition, equivalent to ten percent of units in the proposed Del Mar Mesa project, for occupancy by, and at rates affordable to households earning no more than 65 percent of median area income, adjusted for household size, located on-site or off-site within the City boundaries, as certified by the San Diego Housing Commission. The affordable units must remain affordable for the life of the unit and should be phased proportionate to development of the market-rate units within the Del Mar Mesa project; or
- b. Dedication of land of equivalent value to a. above, located within the City boundaries, as certified by the San Diego Housing Commission; or
- c. Developers may, at the discretion of the City, satisfy the requirements of the FUA affordable housing program by paying an in-lieu fee to the City's NCFUA Affordable Housing Trust Account an amount of money equivalent

to the cost of achieving the level of affordability required by the Del Mar Mesa affordable housing program as determined by the San Diego Housing Commission. The in-lieu fee requirement shall be included as a tentative map condition, where applicable and collected at the time of issuance of building permits; or

- d. Developers of projects within Del Mar Mesa of ten or fewer units, or for larger projects subject to the restrictions specified below, seeking to fulfill the requirement on-site, may do so through the provision of accessory unit(s). Accessory units within Subarea V shall be subject to the CUP requirement and development standards in Municipal Code Section 101.0512 or subsequent amendments. However, the provisions that CUPs only be issued in R-1 zones, Section F. (Suspension of Companion Unit Regulations) and G. (Exclusion of Companion Units in the Coastal Zone) shall not be applicable to the Del Mar Mesa Specific Plan area. Restrictions on occupancy of these units in the above code section may be deviated from subject to certification by the Housing Commission. It is the intent of this specific plan that occupancy of these units be limited to eligible low-income households earning no more than 65 percent of median area income at affordable rental rates. For projects greater than ten units, the number of accessory units shall not exceed 30 percent of the above affordable housing requirement.
- e. Within the Coastal Zone, increased density achieved through provisions of state law mandating density bonuses shall not result in greater encroachment in designated Resource Based Open Space or augmentation of any circulation element road beyond what is adopted in the Plan.

4. Horses and Horse Stables

The Del Mar Mesa community is one of the few areas that is planned to remain rural or semirural in the City of San Diego. The Plan and the underlying zoning requires that the community develop consistent with agricultural zoning. The primary uses and amenities in Del Mar Mesa will consist of single-family residential on larger lots, hiking and equestrian trails, horse stables and a resort hotel and golf course. Currently, the community has scattered single-family residences, horse stables, a series of informal hiking and equestrian trails and large open spaces. Many of the residents of Del Mar Mesa keep horses on their property and it is anticipated that some of the residents of the new developments will desire to keep horses on their properties or in the stables that are located in the community. The ability to maintain horses on residential property is unique in the City of San Diego and should be preserved in rural communities such as Del Mar Mesa.

The City of San Diego Municipal Code contains regulations for the keeping of horses in Section 44.0308. Horses shall be permitted in Del Mar Mesa as long as they comply with these regulations, which state the following:

No person shall bring or maintain within the City any horse, unless

- (a) the number of dwelling units within a one-fourth mile wide belt surrounding the corral, pasture or stable within which the horse is kept, is less than 300 units; and*
- (b) 10,000 square feet of such stable, pasture area or corral or combination thereof is provided for up to two (2) horses with an additional 5,000 square feet for each horse in excess of (2); and*
- (c) no residence or dwelling unit exists except such as are owned, maintained or occupied by the owner of such horses within a 75-foot wide belt surrounding the stable, corral or pasture within which such horse is kept.*

In order to reduce the conflicts that may be associated with future development, all projects shall disclose to prospective residents that Del Mar Mesa is a semirural community which allows residents to maintain horses and/or horse stables on their property.

5. Resort Hotel and Golf Course

On March 26, 1996, a majority of the voters of the City of San Diego approved Proposition C, which amended the City's General Plan to allow the City Council to consider a request for a Conditional Use Permit (CUP) for a 300-room resort hotel in the community of Del Mar Mesa. The CUP was approved by the City Council and the resulting project consists of a 300-room resort hotel, an 18-hole golf course and 134 residential dwelling units.

B. MULTIPLE SPECIES CONSERVATION PROGRAM (MSCP)/OPEN SPACE

Since the adoption of the Framework Plan (and the Environmental Tier) in 1992, the City's adopted resource planning efforts largely involved the development of the MSCP. As a regional habitat conservation plan, the MSCP was designed to address habitat needs for multiple species and includes a preserve designed to address the preservation of natural communities for an 885-square-mile area in the jurisdictions of 11 cities and a sizeable portion of the unincorporated County of San Diego.

The ecosystem of southwestern San Diego County consists of a diverse assemblage of vegetation communities/habitat that supports a wide array of plant animal species. These native vegetation communities are considered sensitive by federal, state and local jurisdictions because they have been severely reduced in distribution as a result of urbanization. Some of these vegetation communities occur only within San Diego as well as the U.S. distribution of component sensitive plant and animal species. Implementation of the MSCP is expected to maintain biodiversity throughout the San Diego area, minimize or avoid species extinction and create a new, streamlined process for the issuance of federal and state permits.

Del Mar Mesa is included in the Northern Area of the City's MSCP Subarea Plan. It is part of the Los Peñasquitos Lagoon and Canyon/Del Mar Mesa core biological area. This core biological resource area encompasses one of the few intact natural open space areas in coastal San Diego County that is still linked to larger expanses of habitat to the east, hence, its tremendous significance. Del Mar Mesa contains core habitat area on the Del Mar Mesa north of the Los Peñasquitos Canyon Preserve in addition to linkages containing disturbed lands and habitat leading toward Carmel Valley and Carmel Creek.

The Plan is consistent with the MSCP Plan and incorporates the MHPA (see **Figure 6**). The Del Mar Mesa open space system is shown on **Figure 7** and on **Exhibit A**, which was adopted concurrently with the specific plan.

The Plan designates the core biological resource areas and sufficient wildlife linkages for preservation within the MHPA of the MSCP (**Figure 6**). Additionally, the Plan identifies four categories of open space based on conservation potential and planned use (**Figure 7**). The open space categories area described below:

1. Publicly-Owned Open Space

At the time that the Plan was adopted in 1997, approximately 551 acres were owned by the City, County, federal or California Department of Transportation (Caltrans) within Del Mar Mesa. This property is designated as Resource Based Open Space in the Plan.

Since the original adoption of the Plan in 1997, additional land or land rights have been purchased for the purposes of conservation and mitigation.

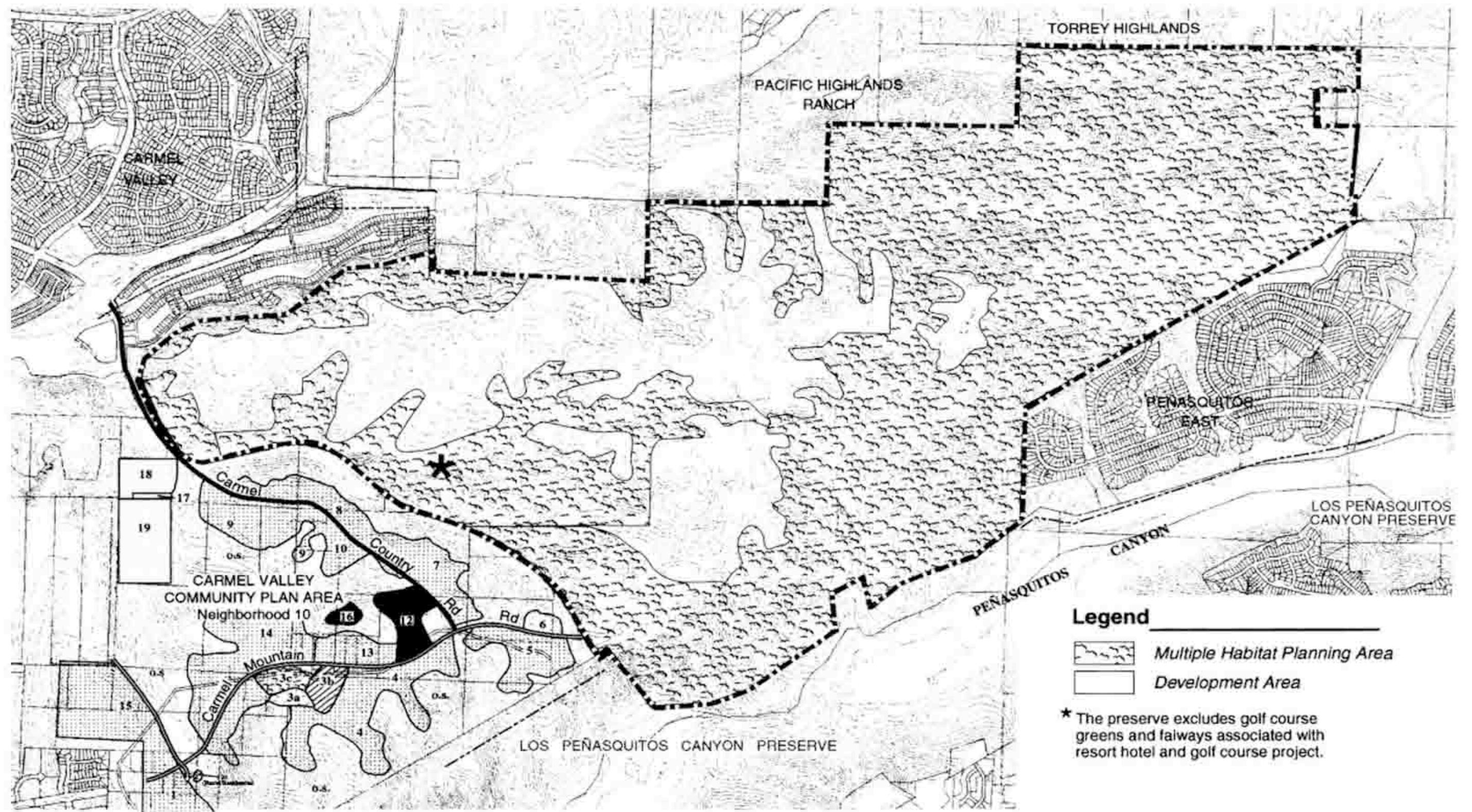
Approximately 95 acres have either been purchased or conserved with easements for conservation and mitigation. This includes approximately 52 acres purchased by the U.S. Fish and Wildlife Service, approximately 18 acres acquired by the City of San Diego and approximately 20 acres conserved by private entities to mitigate for off-site impacts.

The majority of this land is comprised of several large contiguous parcels which form the entire southeastern border of the subarea. Because this area is adjacent to the Los Peñasquitos Canyon preserve, it serves visually and biologically as an extension of this preserve and adds substantially to this regionally significant open space system.

A portion of the open space in Del Mar Mesa also lies within the San Diego National Wildlife Refuge and contains highly sensitive vernal pool habitat. The purpose of the National Wildlife Refuge is to protect and manage key native habitats for several endangered, threatened and rare vernal pool species. A portion of the Refuge is concurrently in public ownership and additional lands may be purchased from willing sellers.

2. Open Space/Rural Residential Area

A total of 388 acres are designated in the easternmost portion of Del Mar Mesa as Open Space/Rural Residential (see **Figure 7**). This area is contiguous to the previously acquired open space lands and is proposed for acquisition, thereby further extending the major open space block which encompasses the entire eastern half of the Del Mar Mesa. When all lands are acquired, this area will represent the largest undisturbed native environment in the entire 12,000-acre NCFUA. These parcels are proposed for acquisition in the Framework Plan and, because of their combined size and undisturbed quality, may be the single most important component within the entire open space system within the NCFUA. More detail regarding the open space acquisition program is contained in the **Implementation** section of this Plan.



Multiple Habitat Boundaries
Del Mar Mesa Specific Plan

6

FIGURE

**TABLE 4
OPEN SPACE SUMMARY**

| OPEN SPACE AREAS | ACREAGE |
|--|-----------------------|
| Publicly Owned Land ¹ | 551.4 ac ¹ |
| Open Space/Rural Residential | 388.5 ac |
| Open Space Acquisition (Exactions/Mitigations) | 330.5 ac. |
| Urban Amenity Open Space | 208.6 ac. |
| TOTAL | 1479.0 ac. |

1. Since the adoption of the Del Mar Mesa Specific Plan, additional land has been purchased for the purposes of conservation and mitigation. Approximately 95 acres have either been purchased or conserved with easements for the purposes of conservation and mitigation, including approximately 52 acres purchased by the U.S. Fish and Wildlife Service, approximately 18 acres acquired by the City of San Diego and approximately 20 acres conserved by private entities to mitigate for off-site impacts.

3. Open Space Acquisition Area - Exactions/Mitigation

As shown on **Figure 7**, there are some areas designated for open space on the western half of Del Mar Mesa which are also designated for proposed acquisition through the subdivision process. These areas are located on parcels that also have areas designated for development. It is intended that this open space be acquired, or set aside by dedication or easement, with approval of a tentative map for the property subject to the supplemental regulations for resource management in this Plan.

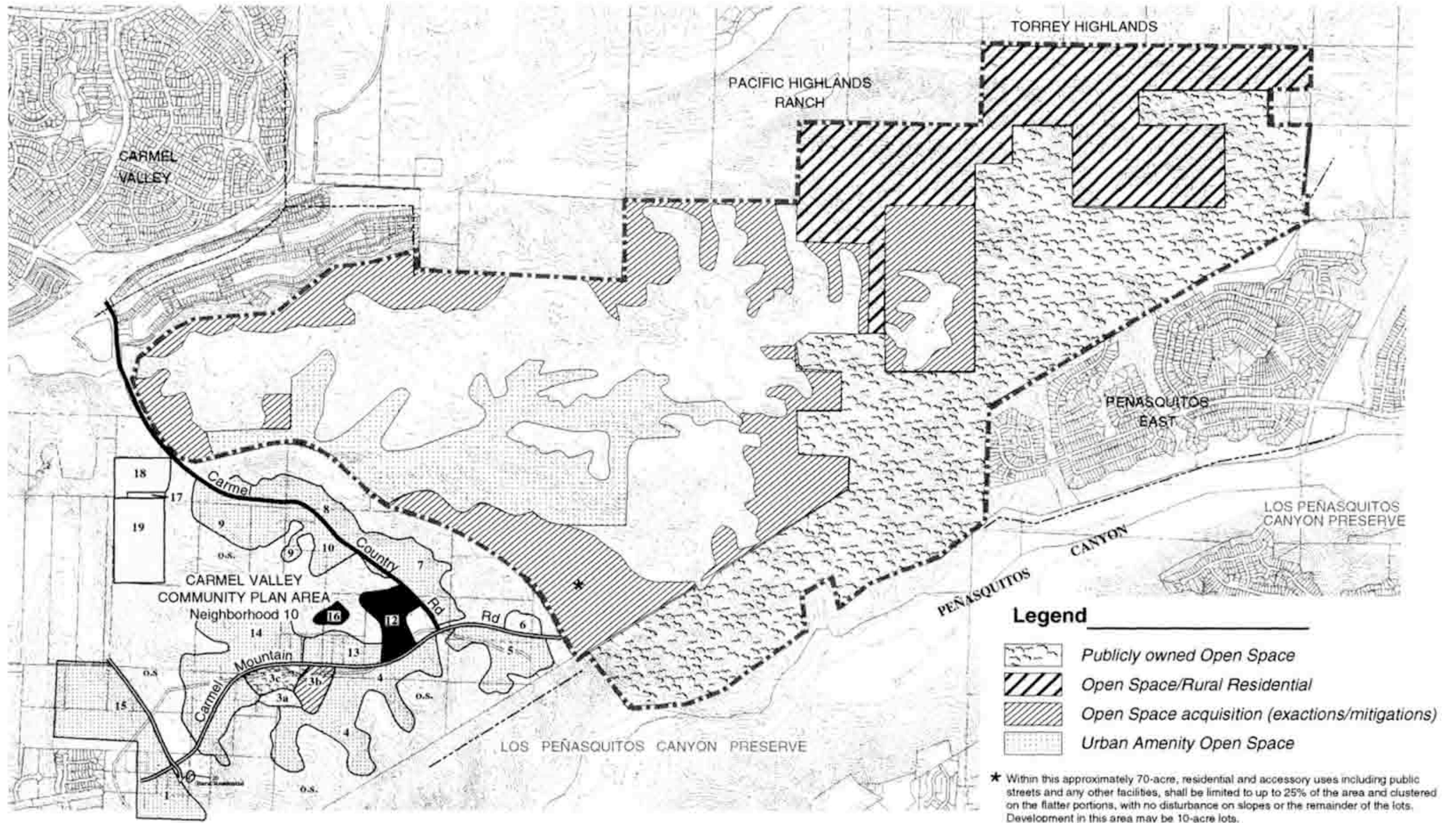
4. Urban Amenity Open Space Areas

The designated Urban Amenity Open Space area through the resort hotel and golf course property and the Shaw property east of Shaw Valley provides a secondary, alternative east-west linkage intended for small wildlife and birds, as well as providing visual relief from adjacent development. A golf course, other recreational and visitor serving amenities, a pond and remaining and restored native vegetation areas proposed on the resort hotel and golf course site are expected to provide for limited wildlife movement and some habitat for native species. Pedestrian paths may be located in the Urban Amenity Open Space on the Shaw property which extends through an under crossing which will be developed below Carmel Mountain Road. This Urban Amenity and under crossing is meant to preserve a secondary link between the areas to the west and those located east of the roadway. Such paths could serve to introduce and educate people to and about the significant native plant and animal species in the area.

TABLE 5
PUBLICLY-OWNED OPEN SPACE SUMMARY

| PUBLICLY-OWNED OPEN SPACE | ACREAGE | | TOTAL |
|--------------------------------------|------------------|---------------|------------------------|
| | AR-1-1 | AR-1-2 | |
| Other Public Agencies/Jurisdictions | 29.2 ac. | 0 ac. | 29.2 ac. |
| County of San Diego | 89.4 ac. | 0 ac. | 89.4 ac. |
| Subtotal | 118.6 ac. | 0 ac. | 118.6 ac. ¹ |
| City of San Diego | 311.3 ac. | 0 ac. | 311.3 ac. |
| Acquired for Mitigation | 101.6 ac. | 20 ac. | 121.5 ac. |
| Acquired with open space bonds | | | |
| Subtotal | 412.9 ac. | 20 ac. | 432.8 ac. ¹ |
| TOTAL | 531.5 ac. | 20 ac. | 551.4 ac. |

1. Since the adoption of the Del Mar Mesa Specific Plan, additional land has been purchased for the purposes of conservation and mitigation. Approximately 93 acres have either been purchased or conserved with easements for the purposes of conservation and mitigation, including approximately 52 acres purchased by the U.S. Fish and Wildlife Service, approximately 18 acres acquired by the City of San Diego and approximately 20 acres conserved by private entities to mitigate for off-site impacts.



5. Multi-Habitat Planning Area Boundary and Criteria for Adjustment

For more specific definition of the Del Mar Mesa open space boundary and MHPA, refer to the 400-scale map adopted as **Exhibit A**. The MSCP Subarea Plan allows the City to make minor adjustments to the MHPA in limited circumstances with subsequent tentative map approvals or other discretionary permit approvals. Minor boundary adjustments will not require amendments to the Plan. Within the Coastal Zone, boundary adjustments which are determined to be consistent with the MSCP implementing agreement between the City of San Diego, the California Department of Fish and Game and the U.S. Fish and Wildlife Service, do not require an amendment to the Local Coastal Program (LCP). The criteria for making these adjustments, is based on whether the resulting change maintains a preserve area that is of equivalent or higher biological value to the original configuration. Within the Coastal Zone, boundary adjustments which require an amendment to the Plan due to modification of **Figure 6** of the Plan will require an amendment to the certified LCP.

Adjustments to the MHPA boundaries may be desirable under some circumstances. For example:

- New biological information is obtained through site-specific studies;
- Unforeseen engineering design opportunities or constraints may be identified during the siting or design of projects that require modification of the MHPA; and/or
- A landowner may request that a portion or all of his property be included within the MHPA.

As provided for in the City's MSCP Subarea Plan, adjustments to the MHPA can be made without the need to amend the MSCP Subarea Plan and the Del Mar Mesa Specific Plan if the adjustment will result in the same or higher biological value of the MHPA. The determination of biological value of the proposed change is made by the City and must have the concurrence of the wildlife agencies. No amendment of the MSCP Subarea Plan is needed for an approved equivalent exchange. The comparison of biological value will be based on the following biological factors:

- Effects on significantly and sufficiently conserved habitats (i.e., the exchange maintains or improves the conservation, configuration, or status of significantly or sufficiently conserved habitats, as defined in the MSCP Plan, Section 4.2.4);
- Effects to covered species (i.e., the exchange maintains or increases the conservation of covered species);
- Effects on habitat linkages and function of preserve areas (i.e., the exchange maintains or improves a habitat linkage or wildlife corridor);
- Effects on preserve configuration and management (i.e., the exchange results in similar or improved management efficiency and/or protection for biological resources);

- Effects on preserve configuration and management (i.e., the exchange results in similar or improved management efficiency and/or protection for biological resources);
- Effects on ecotones or other conditions affecting species diversity (i.e., the exchange maintains topographic and structural diversity and habitat interfaces of the preserve); and/or
- Effects to species of concern not on the covered species list (i.e., the exchange does not significantly increase the likelihood that an uncovered species will meet the criteria for listing under either the federal or state Endangered Species Acts).

6. Guidelines for Development Areas in and Adjacent to the MHPA

The following are specific guidelines for development within the development areas in and/or adjacent to the MHPA in Del Mar Mesa:

- a. The preserve excludes golf course greens and fairways, although these areas may provide for some wildlife movement. The precise layout and configuration of the golf course greens and fairways shall be established when the proposed project is approved by the City of San Diego. Minor adjustments to the preserve in this location, which result in an equivalent or higher biological value, may be accommodated without an amendment of the Plan.
- b. Within the approximately 70-acre area located within the southwest portion of the Shaw property, residential and accessory uses, including public streets and any other facilities, shall be limited to up to 25 percent of the area and clustered on the flatter portions.
- c. For the Shaw Texas property (Area No. 61 on **Figure 30**) and Areas 70, 59 and 44 abutting the MHPA to the east, and extending to the border of the AR-1-2 zoned areas to the north, all brush management shall occur within the defined development area for lots contiguous to the MHPA. This requirement also applies to Area Nos. 9, 32, 23 and 33, abutting the AR-1-2 zoned areas to the east and the MHPA to the north. Deviations from brush management standards shall be considered if they are consistent with the Alternative Compliance provision of the Landscape Technical Manual.
- d. Fencing or other barriers will be used where it is determined to be the best method to achieve conservation goals and adjacent to land uses incompatible with the preserve.
- e. Where grading is necessary, daylight grading at the edges of the preserve is preferred. All grading proposed adjacent to the preserve, including all cut and fill slopes, must occur wholly within the development area, except as specified in the Plan. Graded areas adjacent to open space shall be re-vegetated with native plant species.
- f. A 12 to 14-foot diameter culvert to facilitate wildlife movement shall be provided where Carmel Mountain Road crosses the Urban Amenity Open Space on the Shaw Texas property (Area No. 61 on **Figure 30**). This culvert

is a Development Impact Fee funded improvement in the Public Facilities Financing Plan.

- g. The designated Urban Amenity Open Space area through the resort hotel and golf course property provides an alternative east-west corridor for wildlife movement. If fencing is proposed within the golf course on the property, it should not inhibit wildlife movement through this area. In areas where fencing is appropriate, split-rail type not to exceed four feet in height is recommended.

7. Guidelines for Resource Based Open Space Areas and Adjacent Areas

The City of San Diego MSCP Subarea Plan applies to the Resource Based Open Space areas within Del Mar Mesa which are included in the adopted MSCP (see **Figure 7**). This document should be used in evaluating appropriate uses and development in these areas.

a. Compatible Land Uses

The following land uses are considered conditionally compatible with the biological objectives of the MSCP and thus will be allowed within the City's MHPA/Resource Based Open Space areas:

- Passive recreation
- Utility lines and roads in compliance with the MSCP Subarea Plan
- Limited water facilities and other essential public facilities
- Limited low-density residential uses
- Limited agriculture

Development on private property designated Open Space/Rural Residential shall not exceed 25 percent of the parcel, consistent with the supplemental regulations for resource management contained in the Plan. Development within these areas that is consistent with the existing zoning, such as single-family residences on lots zoned AR-1-1, is consistent with the Plan. Expansion of existing uses would need to be in compliance with the Plan and should provide measures to minimize impacts on the preserve including lighting, noise or uncontrolled access.

b. Roads and Utilities

- All proposed utility lines (e.g., sewer, water, etc.) should be designed to avoid or minimize intrusion into the MHPA. These facilities should be routed through developed or developing areas rather than the MHPA, where possible. If no other routing is feasible, then the lines should follow previously existing roads, easements, rights-of-way, and disturbed areas, minimizing habitat fragmentation.
- All new development for utilities and facilities within or crossing preserve areas shall be planned, designed, located and constructed to minimize environmental impacts. All such activities must avoid disturbing the habitat of MSCP covered species and wetlands. If avoidance is infeasible, mitigation will be required.

- Temporary construction areas and roads, staging areas, or permanent access roads must not disturb existing habitat unless determined to be unavoidable. All such activities must occur on existing agricultural lands or in other disturbed areas rather than in habitat. If temporary habitat disturbance is unavoidable, then restoration of, and/or mitigation for, the disturbed area after project completion will be required.
 - Construction and maintenance activities in wildlife corridors must avoid significant disruption of corridor usage. Environmental documents and Mitigation Monitoring and Reporting Programs pertaining to such development must clearly specify how this will be achieved, and construction plans must contain all the pertinent information and be readily available to crews in the field. Training of construction crews and field workers must be conducted to ensure that all conditions are met. A responsible party must be specified.
 - Roads in the preserve will be limited to those identified in the Plan, roads necessary for maintenance and emergency access and local streets needed to access isolated development areas.
 - Development of roads in canyon bottoms should be avoided whenever feasible. If an alternative location outside the preserve is not feasible, then the road must be designed to cross the shortest length possible of the preserve in order to minimize impacts and fragmentation of sensitive species and habitat. If roads cross the preserve, they should provide for fully functional wildlife movement capability. Bridges are the preferred method of providing for movement, although culverts in selected locations may be acceptable. Fencing, grading and plant cover should be provided where needed to protect and shield animals, and guide them away from roads to appropriate crossings.
 - Where possible, roads within the preserve should be narrowed from existing design standards to minimize habitat fragmentation and disruption of wildlife movement and breeding areas. Roads must be located in lower quality habitat or disturbed areas to the extent possible.
- c. Fencing and Lighting
- Fencing or other barriers will be used where it is determined to be the best method to achieve conservation goals and in areas adjacent to land uses incompatible with the MHPA. For example, chain link or other appropriate fencing material may be used to direct wildlife to appropriate corridor crossings, and natural rock/barrier or split-rail fencing to direct public access to appropriate locations and away from sensitive species or habitats (e.g., vernal pools). For a description of the appropriate fencing designs and materials in Del Mar Mesa, refer to the **Community Design Element** of this Plan.
 - In order to preserve the rural character and the dark night skies characteristic in Del Mar Mesa, standard street lighting will not be provided in the community. Those lights that are required for safety as

determined by the City Engineer shall be designed to avoid intrusion into the MHPA, and to reduce negative effects on wildlife. Lighting in areas of wildlife crossings should be of low-sodium or similar lighting. Lighting of all developed areas adjacent to the MHPA should be directed away from the preserve. Where necessary, development should provide adequate shielding with non-invasive plant materials (preferably native), berming, and/or other methods to protect the MHPA and sensitive species from night lighting.

C. COMMUNITY FACILITIES

1. Schools

With regard to elementary schools, the western portion of Del Mar Mesa is within the Del Mar Union Elementary School District (DMUSD) and the eastern portion of the subarea is within the Poway Unified School District (PUSD) (see **Figure 8**). Junior and Senior High School education is provided by the San Dieguito Union High School District (SDUHSD). Since this Plan anticipates little or no residential development in the eastern portion of the subarea, most students will attend schools provided by the Del Mar and San Dieguito school districts.

Based on a projected build-out of 685 single family dwelling units, Del Mar Mesa is expected to generate approximately 320 elementary school students, 77 junior high school students, and 152 high school students.

**TABLE 6
PROJECTED STUDENT GENERATION**

| DISTRICT | ELEMENTARY | JUNIOR HIGH | HIGH SCHOOL |
|---------------------------------|------------|----------------|----------------|
| Del Mar Union ¹ | 312 | — | — |
| Poway Unified ² | 8 | 4 | 6 |
| San Dieguito Union ³ | — | 73 | 146 |

1. Based on an elementary student generation rate of 0.471 students per dwelling unit.
2. Based on elementary, junior and senior high school student generation rates of 0.34, 0.18 and 0.26 students per dwelling unit respectively.
3. Based on junior and senior high school student generation rates of 0.11 and 0.22 students per dwelling unit respectively.

a. Elementary Schools

In accordance with DMUSD standards, residential development within the Del Mar Mesa area will result in the need for a new elementary school when 300 students are projected to be un-housed in the succeeding school year. As projected development in the Del Mar Mesa area is anticipated to generate over 300 elementary students within the service area of the DMUSD, an elementary school site is designated on **Figure 5**. This site falls within areas 44 and 59 on **Figure 30**. Ownership area number 70 is identified as an alternative location for a joint school/park site.

Until sufficient students have been generated from this and adjacent areas, and sufficient mitigation payments, special taxes, or other funds are collected to fund the property acquisition and development, the identified school/park site property shall retain development rights consistent with similarly zoned parcels in the Plan, or one dwelling unit per 2.5 gross acres,

except in cases where the density is further defined in the Plan. If, prior to acquisition by the DMUSD and/or City of San Diego, the property owner makes application for a subdivision of land or other discretionary action, the City and the DMUSD shall have the opportunity to negotiate purchase of the identified property. If the DMUSD and/or City of San Diego is unsuccessful in securing the school/park site, a similar process shall apply to the alternative location. Dwelling units assigned to parcels identified as the primary or alternative locations for a joint school/park site may be transferred by use of a PDP to other parcels in the NCFUA owned by the same entity.

Developed in conjunction with a neighborhood park, a joint school/park facility will occupy a minimum of thirteen flat net usable acres, with five acres used exclusively for the school, five acres of joint-use playing fields and three acres operated separately by the City. Until sufficient students have been generated from this and adjacent areas, and sufficient mitigation payments, special taxes, or other funds are collected to fund the property acquisition and development, elementary students within the DMUSD service area will attend existing schools within DMUSD. The school facilities financing plans include provisions for the funding of temporary facilities to accommodate additional students prior to the construction of new permanent facilities.

The threshold for construction of a new elementary school within the PUSD is not met by anticipated development in the Del Mar Mesa area.

Elementary students residing within the PUSD service area will attend Deer Canyon Elementary School in Rancho Peñasquitos.

Elementary school financing is addressed in Section VI. G. of this Plan.

b. Junior and Senior High Schools

The thresholds for new junior and senior high schools are not met by anticipated development in Del Mar Mesa. However, the cumulative impacts of projected development within the region will necessitate construction of a new junior and/or senior high school at a future date. Until additional facilities are constructed, most students residing in the Plan area will attend Carmel Valley Middle School in Carmel Valley and Torrey Pines High School in Carmel Valley. Students residing within the boundaries of the PUSD will attend Mesa Verde Middle School in Torrey Highlands and Mount Carmel High School in Rancho Peñasquitos.

Junior and senior high school financing is addressed in Section IV. G. of this Plan.

2. Parks

The projected population of Del Mar Mesa will not, according to General Plan standards, require the construction of a complete ten-acre neighborhood park. However, the population will generate demand for recreational facilities. Therefore, a site of sufficient size to accommodate a joint facility combining a neighborhood park and an elementary school site has been identified. The

facility will occupy a total of 13 acres, with five acres used exclusively for the school, five acres of playing fields used jointly and three acres operated separately by the City. Should the DMUSD determine that an elementary school site within the Plan area is not feasible, a neighborhood park of at least four acres will be required.

In addition to providing for the neighborhood park, developers within Del Mar Mesa will be required to pay an impact fee for partial development of a community park. The community park for the residents of Del Mar Mesa will be located in the community of Pacific Highlands Ranch and is depicted in the Pacific Highlands Ranch Subarea Plan adjacent to the village, civic and transit center uses.

3. Library

The projected population of Del Mar Mesa alone is not sufficient to require a new library. A library is required to serve the developments in the communities of Black Mountain Ranch, Pacific Highlands Ranch, Torrey Highlands and Del Mar Mesa, and is to be located in Pacific Highlands Ranch. Until the Pacific Highlands Branch Library is built, residents of Del Mar Mesa would likely use the Carmel Valley branch library in Neighborhood 9. Developers of Del Mar Mesa are required to pay an impact fee for their fair share of construction of a branch library in Pacific Highlands Ranch.

4. Police

Police protection will be provided by the Northern Division of the San Diego Police Department. The nearest station is located in University City and a new station is planned in Carmel Valley. There is also a storefront site reserved at Black Mountain Ranch should it become needed at a future time.

5. Fire

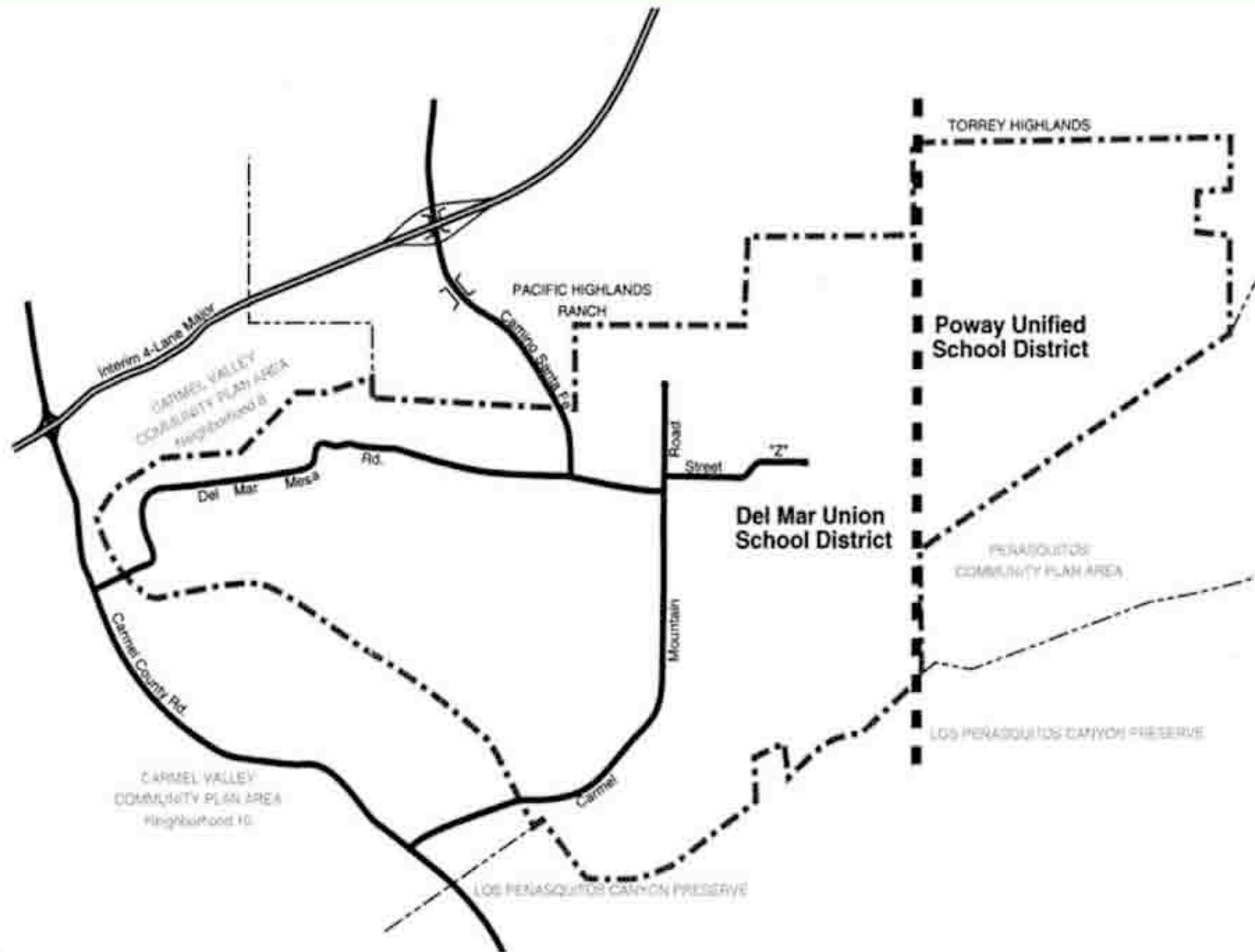
Fire protection will be provided by the San Diego Fire Department. The nearest fire stations are in Mira Mesa and Carmel Valley Neighborhood 7. Construction of two fire stations, the one in Black Mountain Ranch and one in Pacific Highlands Ranch, is planned for surrounding communities. Developers of Del Mar Mesa will be required to pay an impact fee to partially fund construction of these fire stations. Until the new stations are built, Del Mar Mesa will be serviced by existing fire stations. In addition, all new development will be reviewed by the Fire Department for fire safety standards, as the community will contain flammable vegetation posing a moderate to high fire risk to future residents.

6. Solid Waste

Solid waste that would be generated by residents of Del Mar Mesa will be directed to the City's existing sanitary landfills. The City is currently examining alternative landfill sites to expand capacity.

7. Power

Power lines and service will be provided by SDG&E. Major power lines will run underground along Carmel Mountain Road



Elementary School District Boundaries

Del Mar Mesa Specific Plan

8

FIGURE

8. Power

Power lines and service will be provided by San Diego Gas and Electric (SDG&E). Major power lines will run underground along Carmel Mountain Road.

9. Water

Water service within Del Mar Mesa will be provided by the City of San Diego Water Department. Currently, existing water facilities in the area have inadequate capacity to serve new development. Existing water transmission facilities in the area include the Del Mar Heights Pipeline to the north, the Rancho Bernardo Pipeline to the east and the Green Valley Pipeline to the west (see **Figure 9**). The only new transmission facility proposed at this time is the Carmel Mountain Road Pipeline. The new transmission facilities proposed at this time are the 30-inch Carmel Mountain Road Pipeline and the 16-inch Del Mar Mesa Road Distribution Main, both of which will traverse Del Mar Mesa in their respective rights-of-way and appropriate easements.

The Miramar 712/North City 610 Water Study will identify needed water transmission and storage facilities to provide adequate capacity to the undeveloped portions of Carmel Valley: Torrey Hills, Del Mar Mesa, Pacific Highlands Ranch, Torrey Highlands, Black Mountain Ranch and Subarea II.

Applicants for tentative maps will be required to provide water studies showing the proposed water distribution system necessary to serve their developments prior to the approval of final maps. Applicants may request the City to process a water reimbursement agreement(s) to recover the cost of facilities including the study in excess of their pro rata share. The proposed water system shall be designed and constructed to the Water Department's standards. Public facilities, such as park and school sites, must be served by public water and sewer facilities.

10. Sewer

Sewer service will be provided by the City of San Diego Metropolitan Wastewater department. Existing sewer facilities in the vicinity of Del Mar Mesa include the Carmel Valley Trunk Sewer to the north and the Peñasquitos Trunk Sewer to the south (see **Figure 10**). All flows generated from Del Mar Mesa will flow into one of these trunk sewers which flow into the Metropolitan Sewerage System. The existing Carmel Valley Trunk Sewer System does not have the capacity to serve the proposed development in accordance with the Plan.

The Carmel Valley Trunk Sewer construction is to be funded through a Facilities Benefit Assessment. All projects that discharge to this trunk sewer will be charged with that assessment upon pulling building permits. The cost per dwelling unit will be determined with the completion of the Carmel Valley Trunk Sewer Master Plan. The developer will provide a letter of commitment to participate in the Facilities Benefit Assessment District. Additionally, a participation agreement will be entered into for all developments that construct the Carmel Valley Trunk Sewer facilities which exceed their fair share.

Applicants for tentative maps will be required to provide sewer studies showing the proposed sewer system necessary to serve their developments and the drainage basins in which they lay prior to the approval of final maps. Applicants may request the City to process sewer reimbursement agreement(s) to recover the cost of facilities including the sewer study in excess of their pro rata share. All public sewer facilities shall be designed and constructed to the Water Utilities Department's standards. If proposed facilities do not meet the required standards, then such facilities shall be private and constructed to the requirements of the Uniform Building and Plumbing Code. No private mains will be permitted in public streets. The cost of operating and maintaining non-regional public sewer pump stations that serve a single development will be borne by the appropriate homeowners' association or other private entity. Each newly-developed lot will be served with a sewer lateral, if possible. Exemptions may be approved by the senior civil engineer of the wastewater section. All septic systems must be approved and permitted by the County of San Diego Department of Health Services. Adjacent developments shall minimize the number of penetrations into the MCHPA by coordinating sewer facilities, access roads, other utilities and trial locations. This coordination may require modifications to street alignments and project grading.

11. Drainage

Existing drainage facilities adjacent to the Plan area consist of the Carmel Valley Restoration and Enhancement Plan (CVREP) within the SR-56 corridor. These include a detention basin at the east end of Palacio Del Mar and a detention basin on the west side of Carmel Country Road at the Shaw Valley junction. The major drainage courses for the Plan area are divided into three categories: first, is the area north of Del Mar Mesa Road which drains down the canyon into the existing Carmel Valley and Deer Canyons; second, is a smaller drainage in the southeast corner of the Plan area which drains to Peñasquitos Canyon south of Del Mar Mesa; and lastly, is the drainage characterized by those properties which drain to the west within the Shaw Valley.

The backbone drainage system for Del Mar Mesa will consist largely of overland flows in the existing natural drainage courses (see **Figure 11**). This is due to the very low density rural and estate lot character of the subdivision proposed within Del Mar Mesa. It is anticipated that subdivisions would be designed with no net diversion of drainage from one of the major basins to another. In addition, there would be potential internal lakes and water features within the golf course which would additionally function as detention basins, desilting basins and water quality basins.

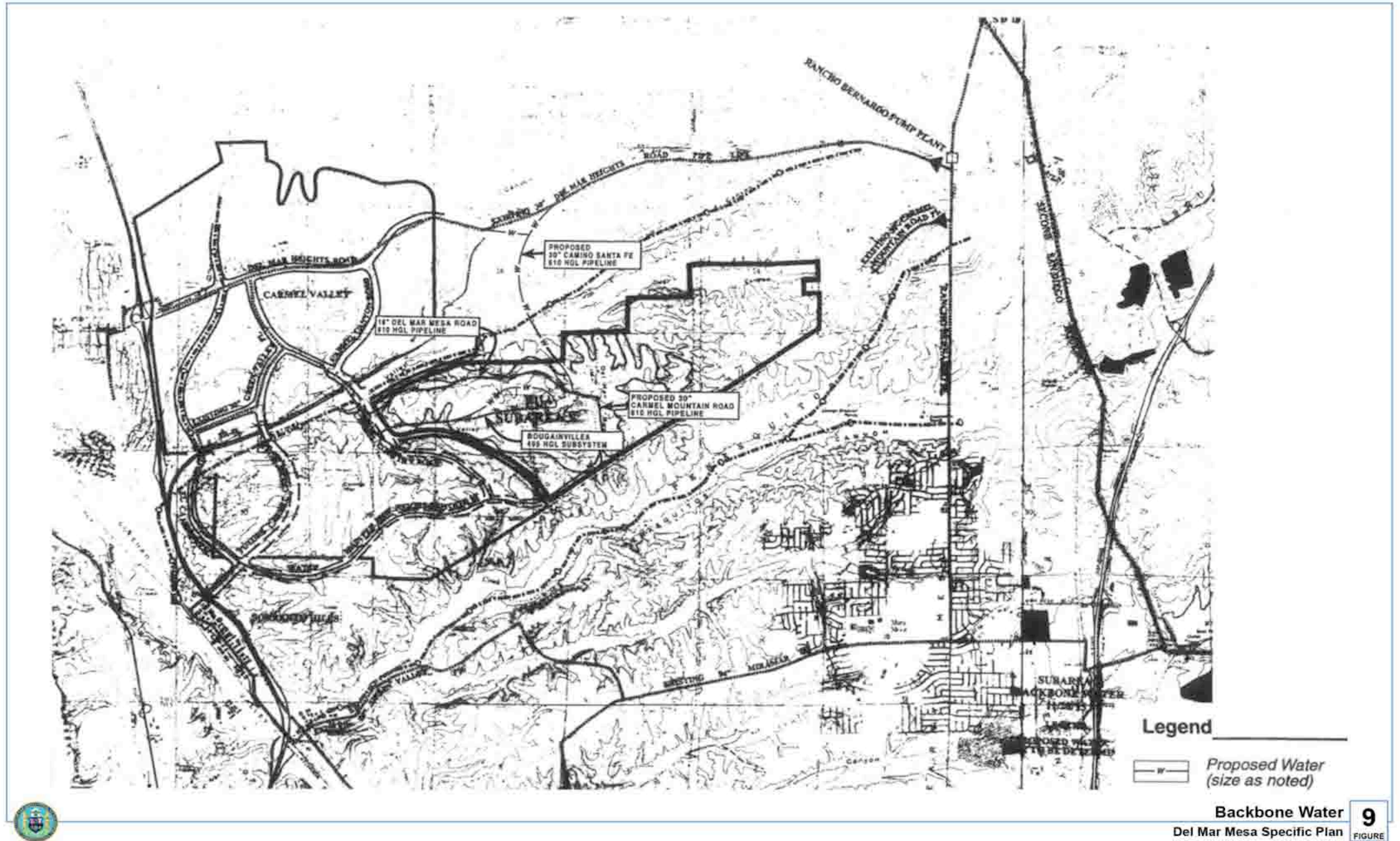
Portions of the community fall within the Coastal Zone boundaries, and as such, proposed drainage solutions would need to meet the criteria identified by the Coastal Commission to prevent siltation and increased run-off from impacting the Peñasquitos Lagoon.

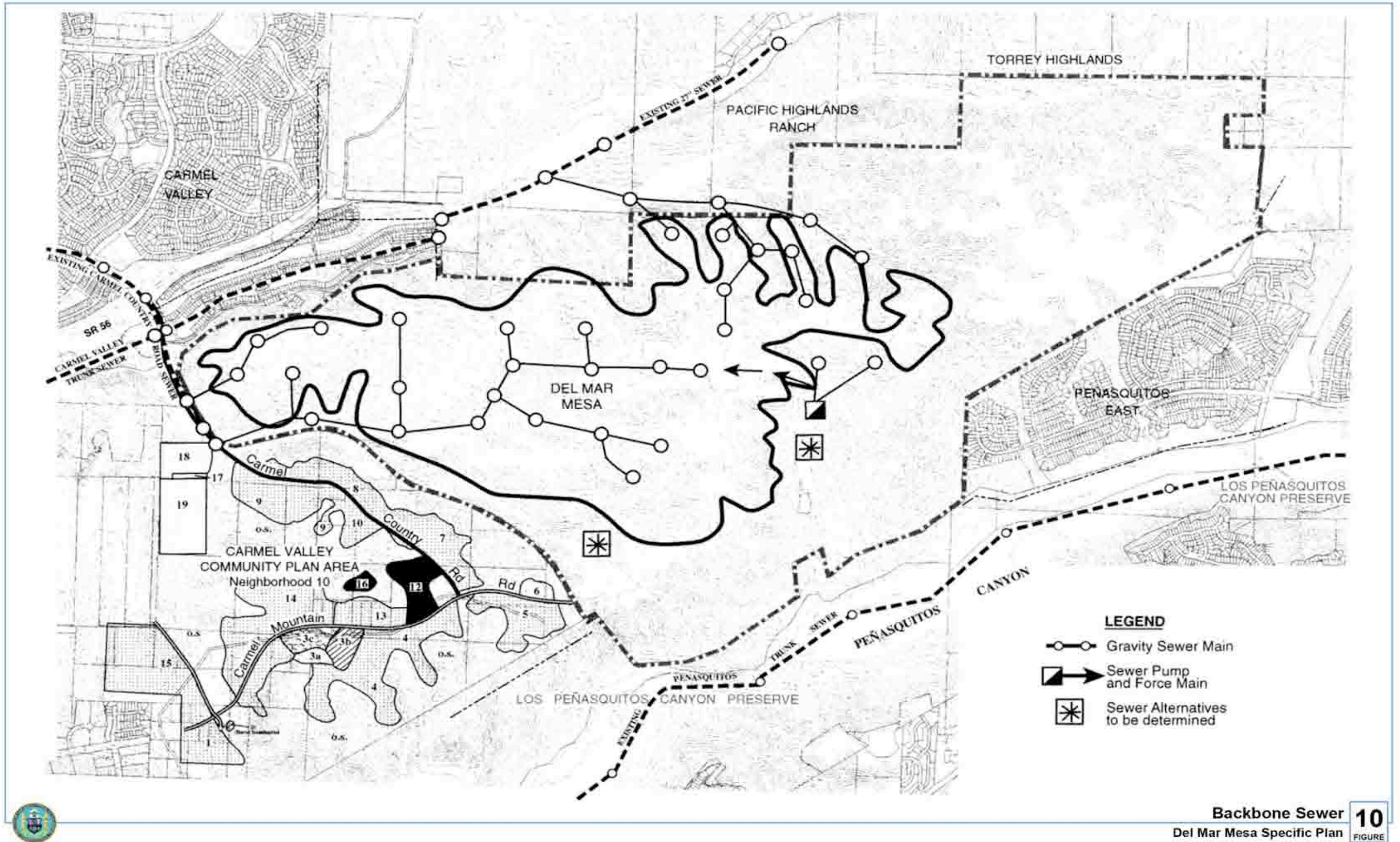
In compliance with the Clean Water Act, "best management practices" (BMPs) may be required to control pollutants and sediment from entering storm water

run-off for the specific plan area. This includes source control BMPs that require landscaping of all manufactured slopes and street right-of-way to prevent erosion and by incorporation of a grading/drainage concept which directs water away from easily erodible areas and into a drainage system designed to safely handle the storm water run-off. Additionally, desilting/water quality basins will be provided at strategic locations within the Plan area as shown on **Figure 11**.

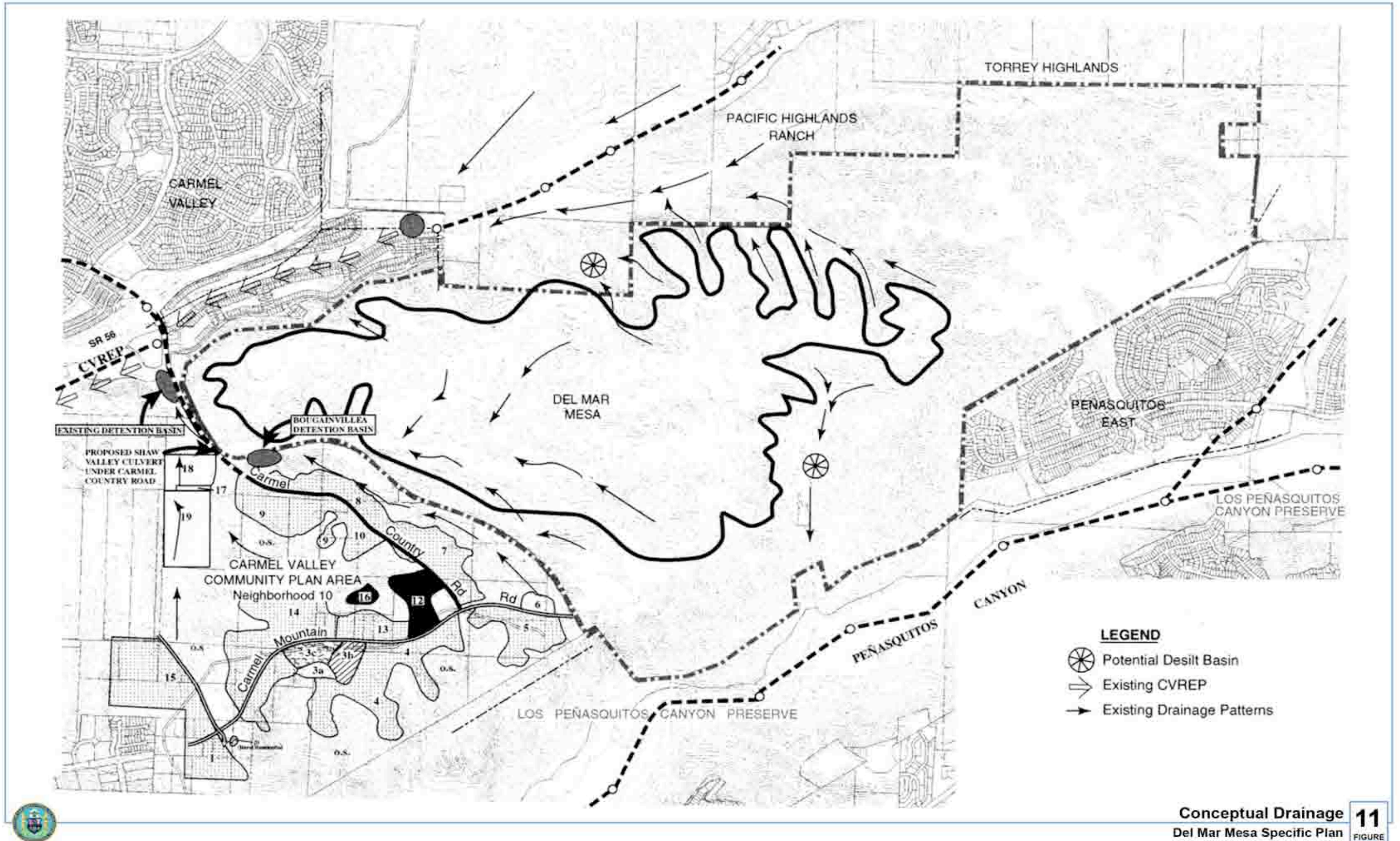
Other applicable BMPs which may be implemented on a citywide basis in conjunction with the City's Municipal National Pollutant Discharge Elimination System permit and the State Regional Water Quality Control Board shall be incorporated into the tentative maps and final plans. The Planning and Development Review Department shall verify that the mitigation measures regarding storm water and drainage management and mitigation of urban run-off flows are conditions for the approval of all subsequent tentative maps within the Plan area.

Prior to, or concurrent with recordation of the first final subdivision map within Del Mar Mesa, a Master Drainage Plan shall be prepared and adopted. This plan shall address sizing and siting of facilities required to mitigate potential impacts to downstream facilities from increases in run-off and erosion, as a result of this specific plan. This Master Drainage Plan shall be comprehensive, covering the entire Del Mar Mesa to the satisfaction of the City Engineer, and shall meet the special requirements for Coastal Zone conformance.





Backbone Sewer
Del Mar Mesa Specific Plan **10**
FIGURE



D. CIRCULATION

1. Introduction

This element addresses the circulation system in Del Mar Mesa. It assumes for the Plan area 685 dwelling units, a 300-room resort hotel and a golf course. The recommendations in this Circulation Element are drawn from the Del Mar Mesa Transportation Study, performed by the City of San Diego Transportation Planning Section in November of 1995, and updated in March 1996.

The major issues related to the street system are proposed improvements to Del Mar Mesa Road and whether the Camino Santa Fe connection from SR-56 to Carmel Mountain Road should be constructed. Other subjects addressed in the Circulation Element are internal streets, driveways, public access, equestrian trails, hiking and pedestrian trails, bicycle circulation, public transit, park-and-ride, parking, street lights and off-road vehicles. The phasing of the recommended transportation improvements is also included in the **Implementation Element** of the Plan.

In order to maintain the rural character of the Del Mar Mesa community, the provision of standard urban street widths and improvements is discouraged. The absence of urban improvements like curbs, gutters, sidewalks and streetlights, help to maintain the rural character of the community. All subdivisions shall incorporate asphaltic berms or rolled curbs, in lieu of concrete curbs and gutters, unless concrete curbs and gutters are required due to drainage considerations. Additionally, graded walkways shall be provided rather than concrete sidewalks. Standard street lighting will not be provided and streetlights shall be provided only in areas in which it is determined by the City Engineer that they are necessary for safety.

2. Guiding Principles

The general guiding principles are those of the North City Future Urbanizing Area Framework Plan. Specific guiding principles are:

- A vehicular and non-vehicular circulation system that meets the needs of Del Mar Mesa residents and visitors at an acceptable level of service.
- An efficient and environmentally sensitive transportation system that maintains the Del Mar Mesa's rural character.
- Hiking and equestrian trails, with access to adjacent trails that provide walking and horseback riding opportunities to the general public and Del Mar Mesa residents.

3. Implementing Principles

The general implementing principles are those of the NCFUA Framework Plan. Specific implementing principles are:

- Street improvements shall be compatible with the rural character of the subarea. Consideration should be given to minimize impacts to the landform, where safety permits and as determined by the City Engineer.
- In order to maintain the rural character of Del Mar Mesa, streets shall be designed with pedestrian and equestrian facilities and rolled curbs.
- Transportation facilities shall be regarded as an integral part of the landscape in which they are located.

4. Existing Conditions

Del Mar Mesa is located in the North City Future Urbanizing Area, east of Carmel Valley Neighborhood 10 and south of SR-56 and Carmel Valley Neighborhood 8. Since Del Mar Mesa is not yet developed, the area is without a paved street system. As shown on **Figure 12**, currently the only roadway that provides access to the existing residences is Shaw Ridge Road, which is an unpaved local road that connects Carmel Country Road to the eastern part of the study area.

In 1998, the name of Shaw Ridge Road was changed to Del Mar Mesa Road to more closely associate the name of this primary artery with the community.

Direct freeway access to the subarea is possible via SR-56 ramps at Carmel Country Road. State Route 56 in this area includes a two-mile stretch of a four-lane freeway from a few hundred feet east of Carmel Country Road to Carmel Valley Road at El Camino Real. Carmel Valley Road has an existing diamond interchange which provides full access to I-5. The construction of a southbound link from SR-56 to I-5 was completed in 1999.

5. Relationships to Other Community Plans

The North City Future Urbanizing Area Framework Plan provides the major guidelines for development of this and other FUA subareas. Carmel Valley Neighborhood 8 is located on the north side and Neighborhood 10 is located on the west side of Del Mar Mesa. Carmel Valley Neighborhood 8A is located west of Neighborhood 10.

The planned street system for Carmel Valley Neighborhoods 8A and 10 directly impacts Del Mar Mesa, as these neighborhoods and Del Mar Mesa would utilize Carmel Country Road for freeway access. The developments on the western side of Del Mar Mesa will access the freeway system from Carmel Country Road which is located in Carmel Valley Neighborhood 10. Construction of this road is included in the Neighborhoods 8A and 10 combined transportation phasing plan. Therefore, the first phase of Del Mar Mesa developments is closely related to developments of this transportation improvement in Carmel Valley Neighborhood 10.

6. Traffic Generation

As shown on **Table 7**, the transportation study for Del Mar Mesa assumed a total of 685 residential dwelling units (DUs), a 300-room resort hotel and a golf course that are expected to generate 9,880 daily trips. The resort hotel project includes a golf course and 134 of the 685 dwelling units. The transportation study also assumed two public projects: a nine-acre neighborhood park that generates 450 daily trips and a four-acre school that generates 240 daily trips for a grand total of 10,570 daily trips. Since the publication of the Del Mar Mesa Transportation Study, the number of dwelling units and distribution of park vs. school acreage have been revised slightly. This does not affect the recommendations presented below or the transportation phasing plan presented in the **Implementation Element** of the Plan.

The phasing of transportation improvements assumes the proposed network (Alternative 3) that includes the Camino Santa Fe connection with the western alignment and Del Mar Mesa Road as a two-lane residential local street.

The transportation network assumes Del Mar Mesa Road as a two-lane residential local street; a Camino Santa Fe connection, between SR-56 and Carmel Mountain Road constructed as a two-lane collector street; and a four-lane major road (as an interim improvement prior to Caltrans' completion of SR-56), from the existing eastern terminus of SR-56 to Camino Santa Fe.

TABLE 7
LAND USE ASSUMPTIONS AND TRIP GENERATION

| LAND USE ASSUMPTIONS | DAILY TRIPS |
|--------------------------------|---------------|
| 685 Dwelling Units | 6,850 |
| 300-Room Resort Hotel | 2,400 |
| 1 Golf Course | 600 |
| Neighborhood Park (9 acres) | 450 |
| Elementary School (four acres) | 240 |
| TOTAL | 10,540 |

Figure 13 shows the proposed street classifications and future traffic volumes.

7. Proposed Circulation System

With the proposed network, Carmel Mountain Road's traffic, east of Carmel Country Road, is projected to be 3,000 daily trips. The projected traffic on Carmel Country Road, north of Carmel Mountain Road, is 5,000 daily trips. The Camino Santa Fe connection is projected to have a future traffic volume of 5,000 daily trips. Based on an ultimate future forecast of 1,200 daily trips, Del Mar Mesa Road will be a two-lane residential local street. Upon construction of the Camino Santa Fe connection to Del Mar Mesa Road, consideration may be given to closing Del Mar Mesa Road to through traffic either by placing a gate or two opposing cul-de-sacs along the roadway. This would maintain the traffic along Del Mar Mesa Road at the level appropriate for a two-lane residential local street. It should be noted that if the Camino Santa Fe connection is not constructed, the projected traffic volume on Del Mar Mesa Road would be about 7,000 daily trips. This will result in Del Mar Mesa Road becoming a defacto collector street. Without the Camino Santa Fe connection, Del Mar Mesa Road could not be closed to through traffic.

A summary of intersection levels of service and lane configuration for key intersections is shown on **Figure 14**. As can be seen in this figure, all the ramps would operate at Level of Service "C" and all the signalized intersections would operate at Level of Service "B."

8. Proposed Future Street Classifications

As noted earlier, the proposed street classifications and traffic forecast for Del Mar Mesa are shown on **Figure 13** and are described below. The general alignments of the proposed street network and classifications are shown on **Figure 15**.

| | |
|-----------------------|---|
| Camino Santa Fe: | Two-Lane Collector street from SR-56 to Carmel Mountain Road. |
| Del Mar Mesa Road: | Two-Lane Residential Local street. |
| Carmel Mountain Road: | |
| Segment 1: | Two-Lane Modified Collector (one lane in each direction with a center left-turn lane where needed) from the Plan area boundary to the open space. |
| Segment 2: | Two-Lane Collector street through the open space. |
| Segment 3: | Two-Lane Modified Collector street north of the open space to Camino Santa Fe. |
| Carmel Country Road: | Four-Lane Major street from SR-56 to south of Neighborhood 10's northern boundary. |

- a. **Del Mar Mesa Road** - As discussed earlier, to prevent Del Mar Mesa Road from becoming a defacto two-lane collector street (i.e., one that is constructed as a two-lane residential local street, however, due to excessive traffic demand would operate as a two-lane collector street), it is recommended that both Del Mar Mesa Road and the Camino Santa Fe connection be constructed. With the Camino Santa Fe connection constructed, the ultimate future traffic volume on Del Mar Mesa Road will be about 1,200 daily trips, which can easily be accommodated by a two-lane residential local street. Due to the proposed alignment of the Camino Santa Fe connection (**Figure 15**), the length of Del Mar Mesa Road will be approximately 1.7 miles. Construction cost for Del Mar Mesa Road is estimated at \$4.4 million, including an eight to ten-foot multiuse trail. **Figure 16** shows the cross section for Del Mar Mesa Road as a two-lane residential local street.

- b. **Camino Santa Fe** - Construction of a Camino Santa Fe connection, between SR-56 and Carmel Mountain Road, was examined as part of alternative analysis for the subarea. **Figure 17** shows the cross section for Camino Santa Fe as a two-lane collector street.
 - The western alignment of Camino Santa Fe is the recommended alignment. This alignment is approximately 1,200 feet west of the eastern alignment. It avoids intrusion into the wildlife corridor and allows larger uninterrupted wildlife habitat. This connection allows another access point to Del Mar Mesa and therefore reduces the subarea's dependence on transportation improvements in Carmel Valley Neighborhoods 8A and 10 which may allow development in Del Mar Mesa to proceed earlier. The cost of the Camino Santa Fe connection between SR-56 and Del Mar Mesa Road is estimated at \$2.8 million.

 - Additional cost associated with the Camino Santa Fe connection includes the bridge over SR-56, estimated at \$1.5 million (to be paid for by the City as part of the SR-56 arterial road construction between Black Mountain Road and Carmel Country Road), and the associated ramps estimated at \$2.5 million to be paid for by the FUA/Del Mar Mesa on a fair share basis.

- c. **Carmel Mountain Road** - As shown on **Figure 15**, the Carmel Mountain Road alignment would begin at the south end of Carmel Country Road and go through the community of Del Mar Mesa. It consists of three segments estimated at a cost of \$5.7 million. All segments will be built with a six-foot parkway and a ten-foot graded but unpaved multiuse trail on one side of the roadway. **Figures 17** and **18** include cross sections for Carmel Mountain Road:

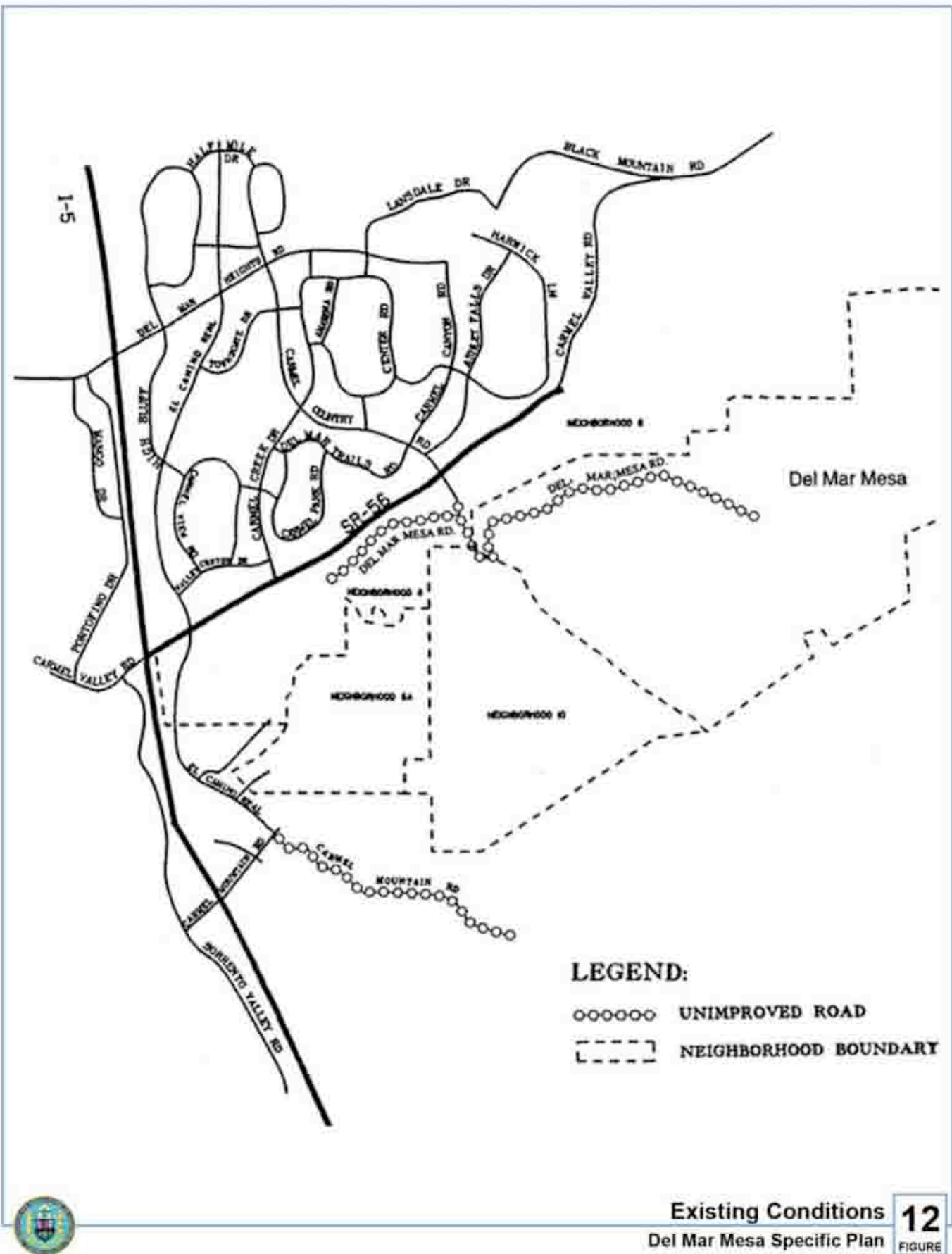
- Segment 1: Will proceed east from the Plan area boundary to the open space. This segment will be a two-lane modified collector street which has one lane in each direction and a center turn lane (50-foot curb-to-curb/72-foot right-of-way), as shown on **Figure 18**.
- Segment 2: Will proceed north-northeast through a primarily open space designated area. The roadway through this area is recommended to be a two-lane collector street (40-foot curb-to-curb/62-foot right-of-way), as shown on **Figure 17**.
- Segment 3: Will proceed northerly from Segment 2 to the Camino Santa Fe connection. This segment is recommended to be a two-lane modified collector street (50-foot curb-to-curb/72-foot right-of-way) which will include one traffic lane in each direction with a center turn lane as shown on **Figure 18**.

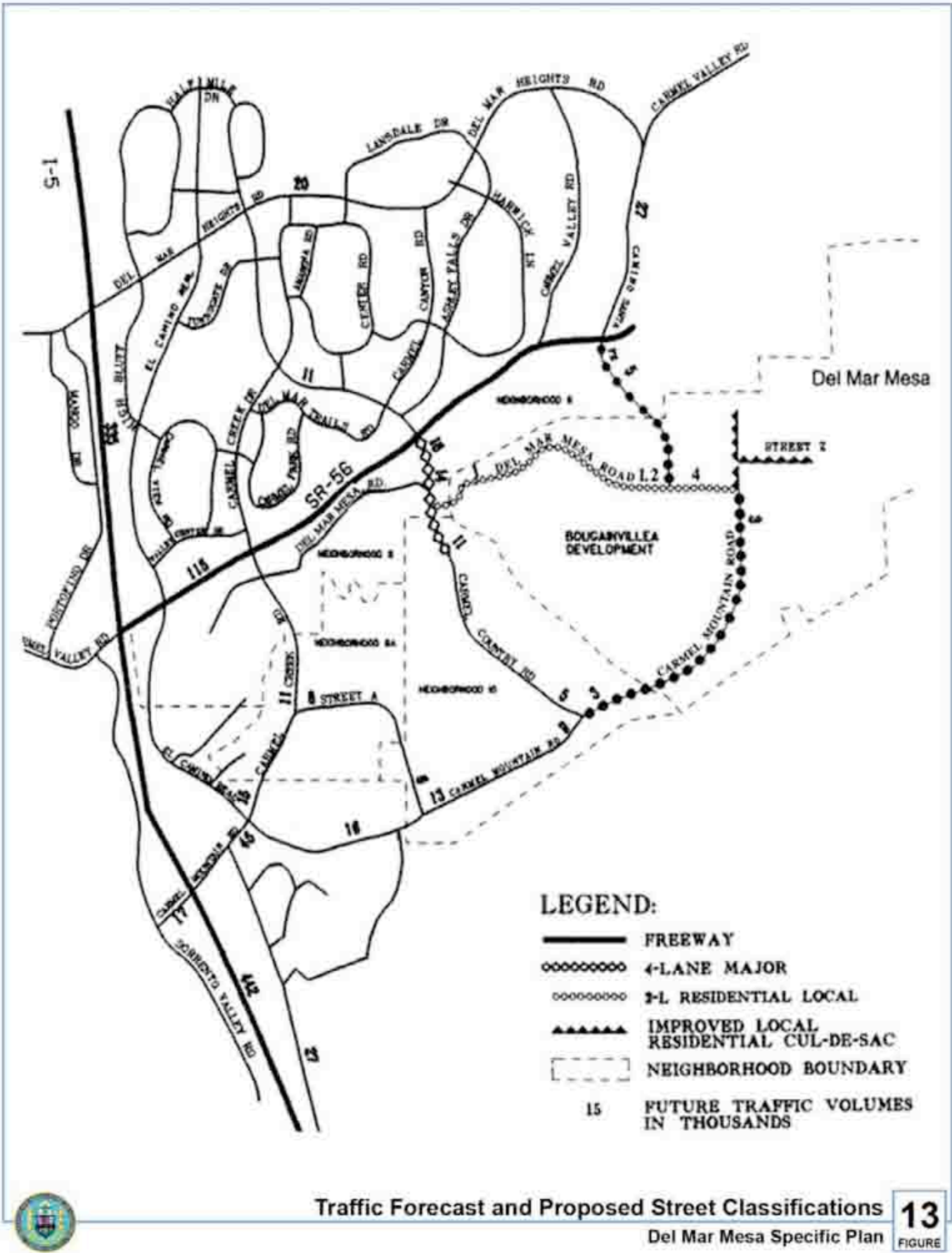
9. Internal Streets

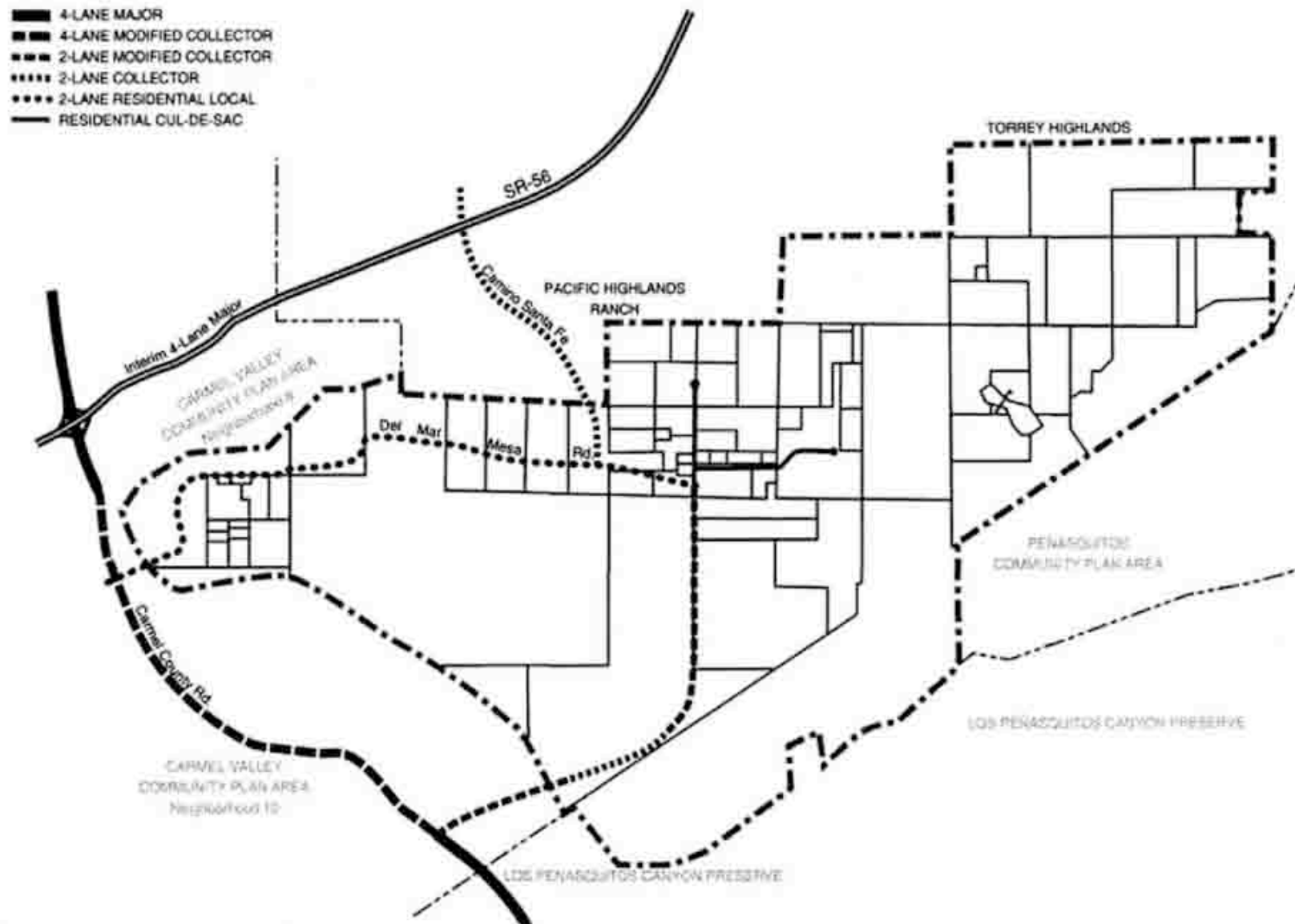
The location of internal local roads will be identified by the individual developers of specific projects at the time of tentative map submittal. These internal roads should be designed to follow the natural contours of the land and minimize the disruption of the existing topography and resources. All proposed internal circulation streets will be subject to the general provisions and guidelines of the Plan, the provisions of the City's Street Design Manual and approval of the City Engineer. Where the provisions of the Plan conflict with the City's Street Design Manual, the Plan standards apply. In order to deviate from the City's Street Design Manual, a deviation request must be made to the City Engineer. Provided that the request conforms to the Plan and the design is determined to be based upon sound engineering and provides for the safety and welfare of the community, the deviation shall be granted through a ministerial procedure. The cross-section for internal local roads is depicted on **Figure 16**.

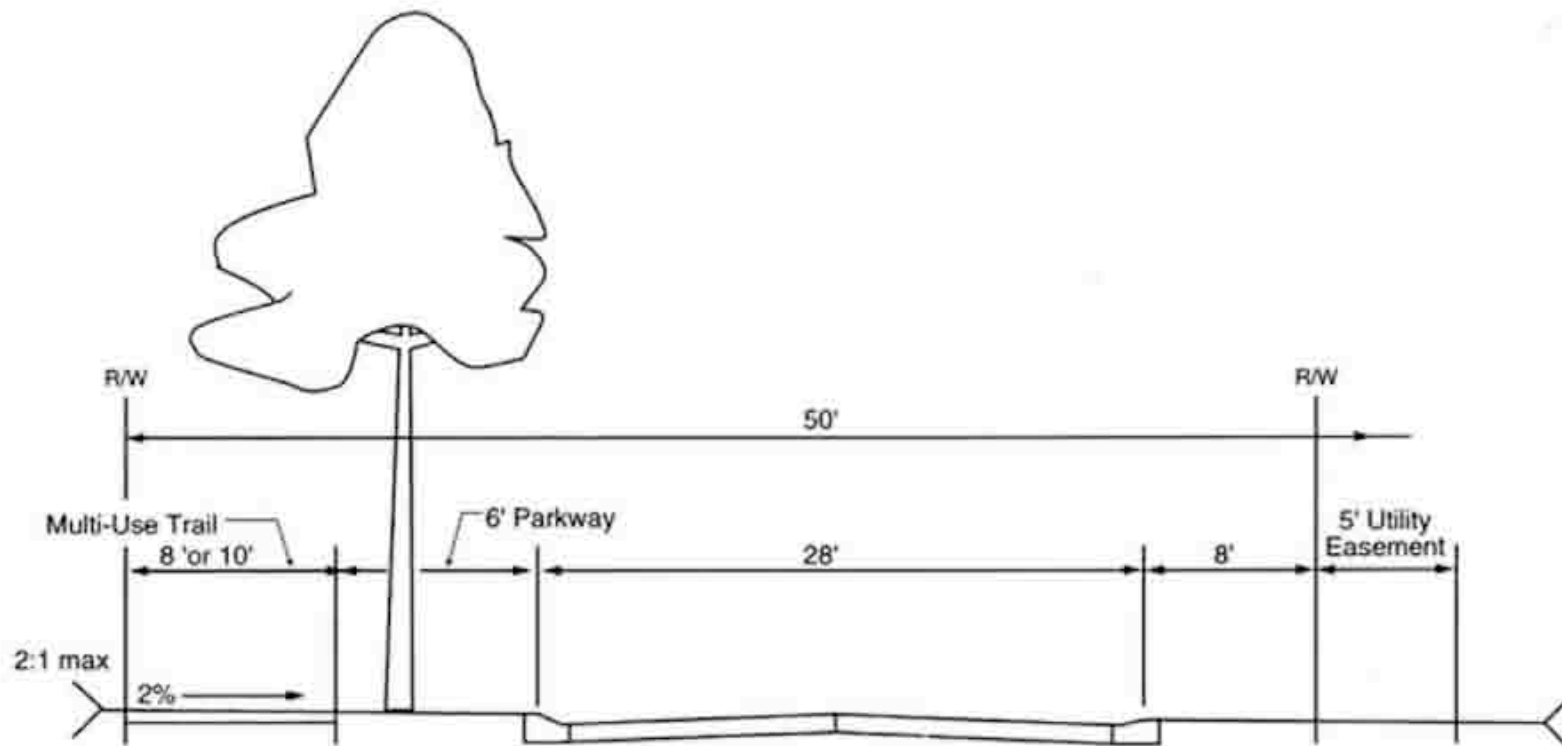
10. Pedestrian Circulation

In an effort to preserve the rural qualities of the Del Mar Mesa community, standard sidewalks shall not be provided along the circulation element roadways or the internal local roadways within the subdivisions. Pedestrian circulation shall be accommodated in the community through the provision of multiuse unpaved trails that are required adjacent to all circulation element roadways and internal local roadways and through a series of hiking and equestrian trails (see **Figure 20**). Examples of appropriate surface treatments are decomposed granite and/or grasscrete or similar materials.









* Where Multi-Use trail and equestrian trail share the same alignment, the joint trail shall be 10 feet wide with 52 feet of right of way.



Residential Local Street and Del Mar Mesa Road

Del Mar Mesa Specific Plan

16

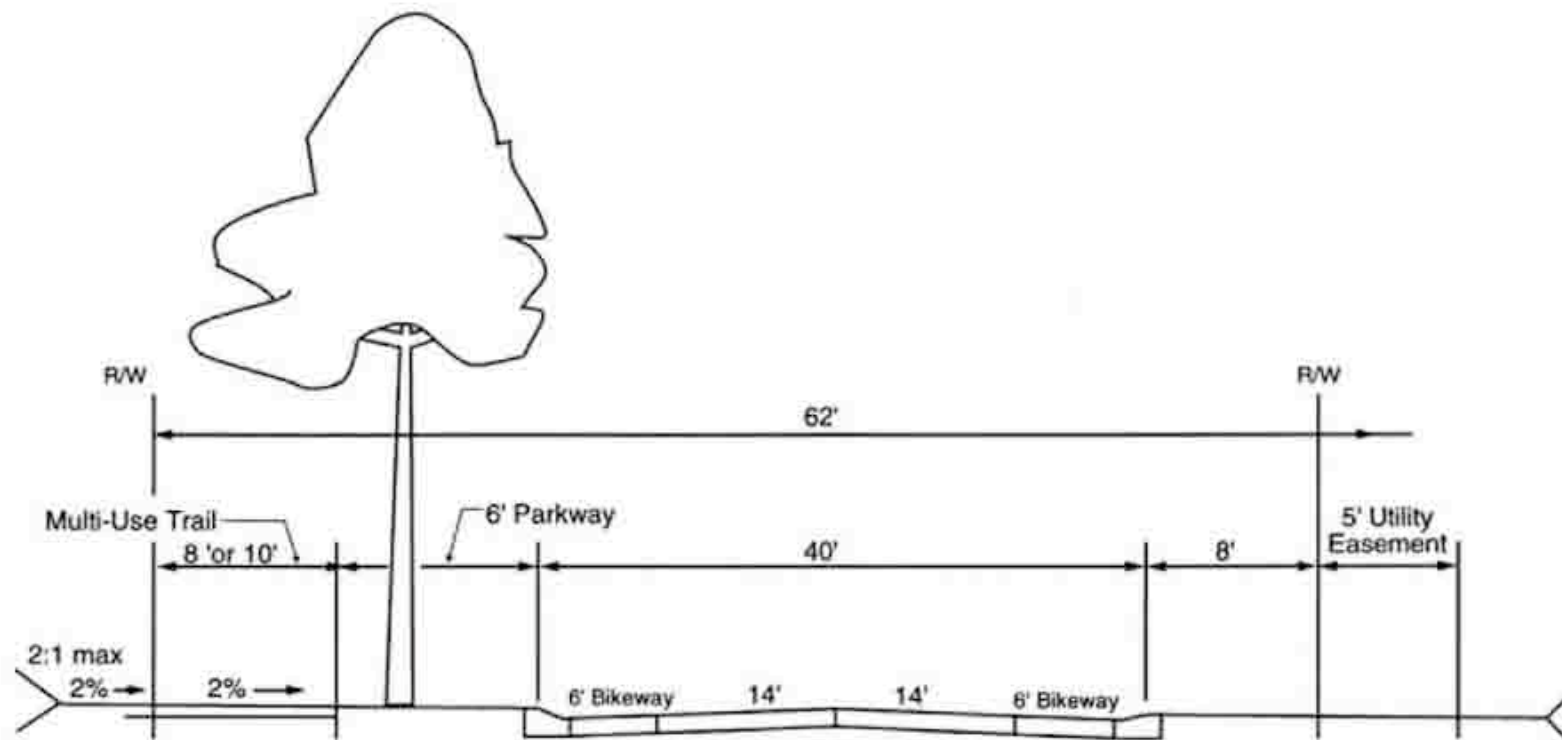
FIGURE

11. Hiking/Equestrian Trails

In order to provide a linkage to the open space system, connect Del Mar Mesa to the surrounding communities and provide the residents in the area with recreational opportunities, the Plan includes two trail systems. The multiuse trail shall be located adjacent to all circulation element roadways and shall be designed to accommodate walking, jogging, bicycling and horse riding activities. The trail shall be ten feet in width and separated from the roadway by a six-foot landscaped parkway. In order to direct trail users and provide for safety, the ten-foot trail shall be separated from the six-foot parkway by a three-foot high split rail type fence (see **Figure 19**). In addition to the multipurpose trail, a hiking/equestrian trail system is proposed. This system is intended to complement the roadside multiuse trail system by providing public hiking and riding opportunities away from vehicular traffic (see **Figure 20**). This system includes a trail on the northwestern edge of Del Mar Mesa connecting to Carmel Valley Neighborhood 8 and provides a link to existing and planned trails in Carmel Valley Neighborhood 10. In addition, trails are identified through the Lorenz Parcel (Area No. 70 on Figure 30) and farther to the east extending from Street Z, following the existing SDG&E easement and linking to Peñasquitos Canyon. The far eastern trail is designated for multiuse and will accommodate mountain bikes.

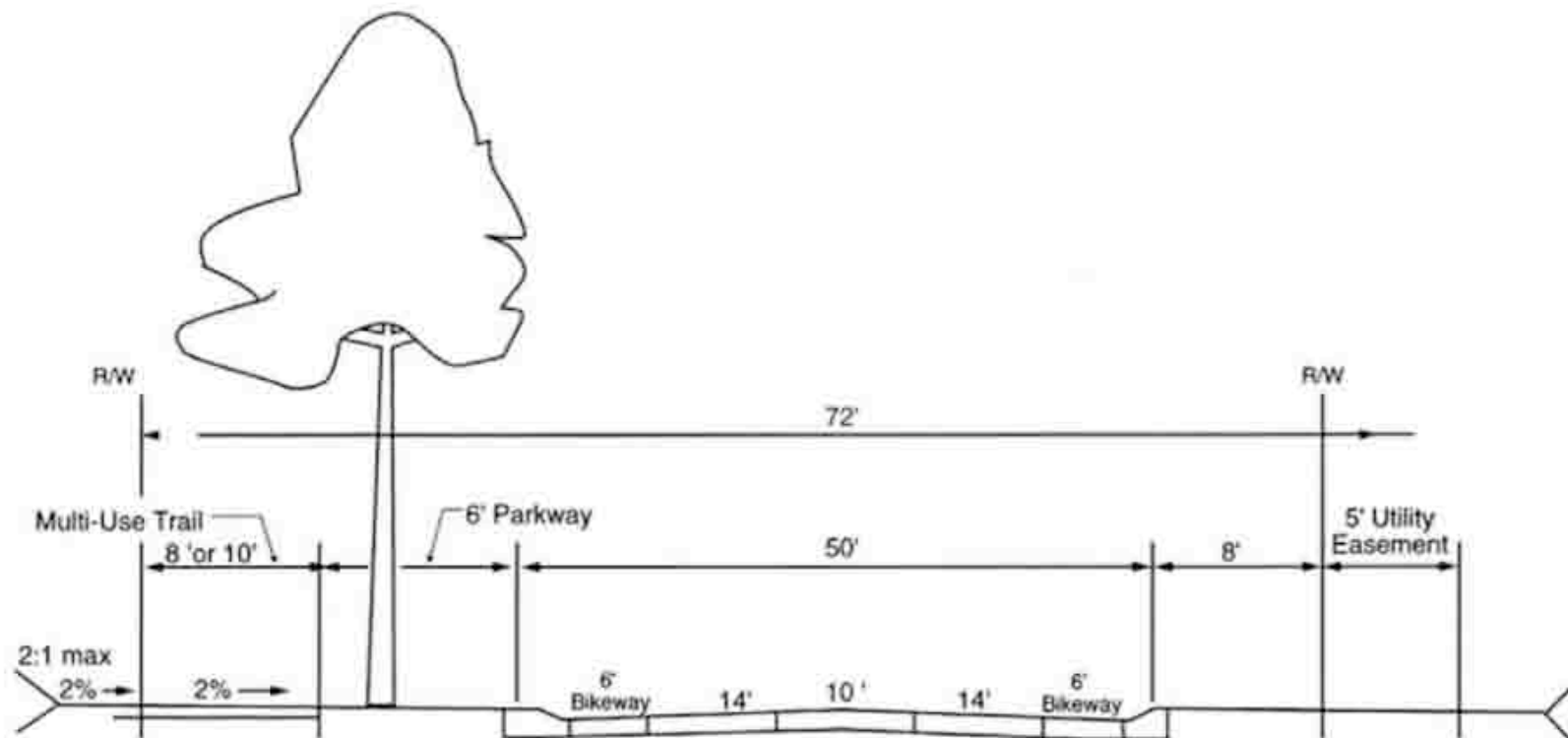
In general, existing equestrian/hiking trails designated for inclusion in the non-vehicular circulation system will be left in their present condition. Limited improvements may be made to address any existing hazards to safe passage. Roadside multiuse trails and new equestrian/hiking trails shall be improved to achieve City trail standards unless the trail is located in the MHPA or in an area with steep topography. Where topographic conditions allow, new trails shall be eight feet in width, constructed of decomposed granite to a depth of six inches and should be no steeper than ten percent grade. Within the MHPA, wildlife corridors and/or in areas of steep topography, trail widths should not exceed four feet in width. The width of the trail shall be ten feet where the multiuse trail and equestrian/hiking trail share the same alignment. Clear signage should be provided to direct users to designated trail areas.

In order to assure the appropriate connections and trail design, a trail plan that implements the goals and objectives of the Plan shall be required prior to the approval of all future tentative maps. With review and approval of subsequent tentative maps within Del Mar Mesa, the precise alignment of the hiking/equestrian trails identified in **Figure 20** shall be determined and secured either through dedication or easement as a tentative map condition. Provisions for the maintenance of common trails shall be made either by defining maintenance as a responsibility of the appropriate homeowner's association in the area or through the formation of a Landscape Maintenance District.



* Where Multi-Use trail and equestrian trail share the same alignment, the joint trail shall be 10 feet wide with 64 feet of right of way.

(No Fronting Property)
Carmel Mountain Road (In Open Space Area)
and Camino Santa Fe Connection



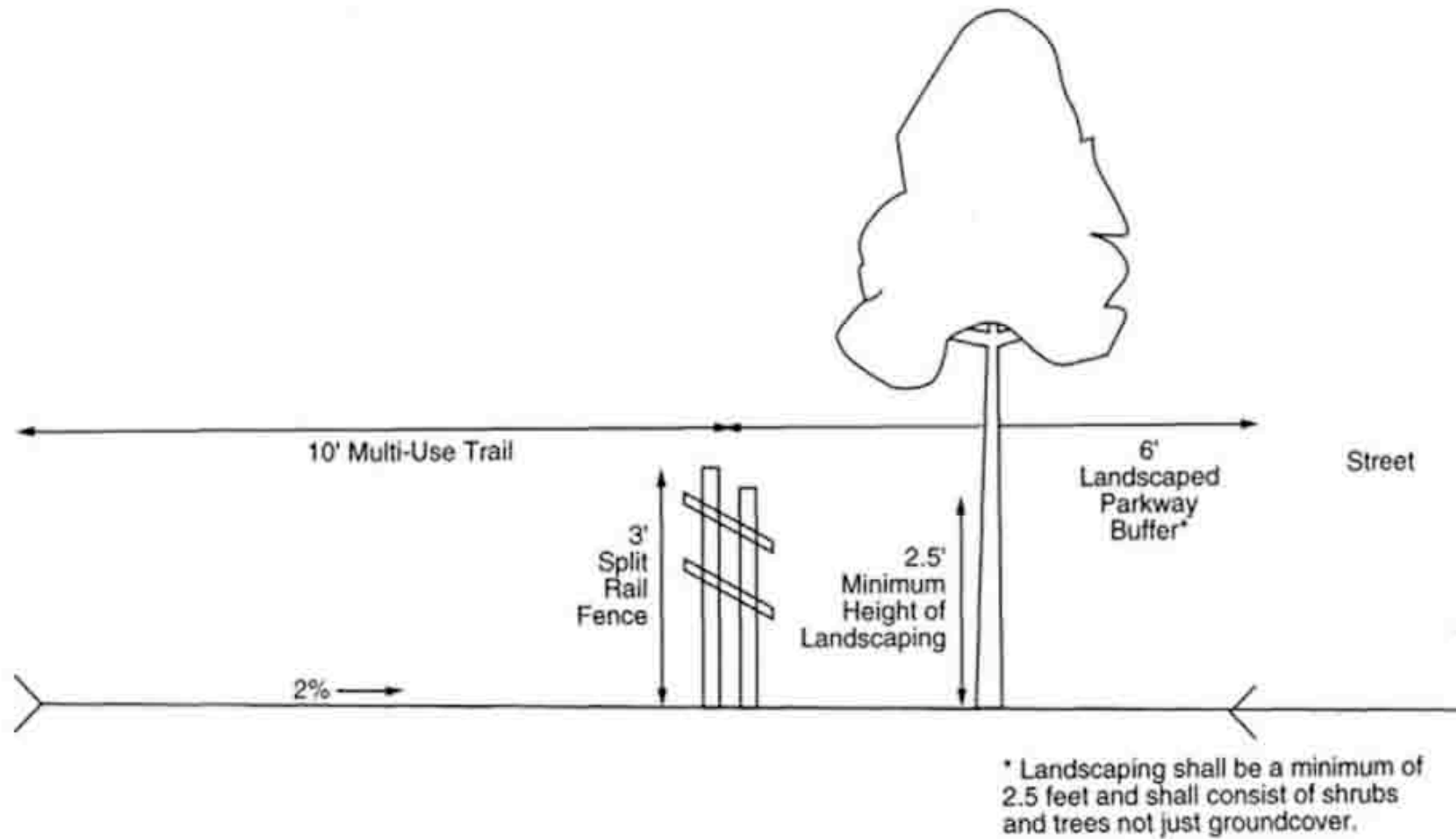
* Where Multi-Use trail and equestrian trail share the same alignment, the joint trail shall be 10 feet wide with 64 feet of right of way.

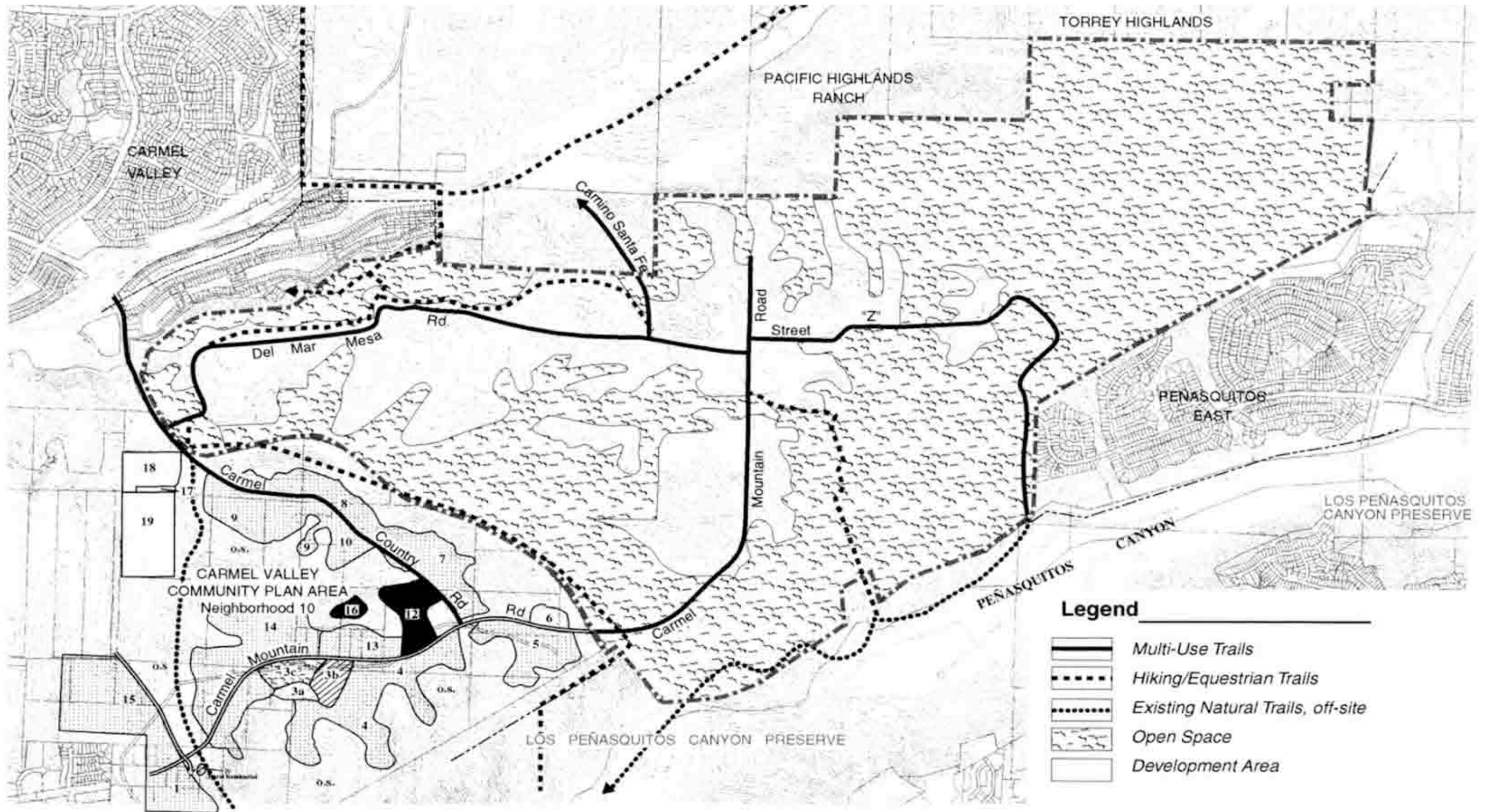
(With a Center Left-Turn Lane)
Carmel Mountain Road



Two-Lane Modified Collector
Del Mar Mesa Specific Plan

18
FIGURE





12. Public Transit

The 1994 Regional Transportation Plan (RTP) identifies the SR-56 corridor between I-5 and I-15 as a potential transit corridor. The RTP states that if development through the North City Future Urbanizing Area is “focused on the potential station areas at sufficient intensities, guideway transit would be cost-effective in this corridor.” Given funding constraints and the proposed low-density development, the Metropolitan Transit Development Board (MTDB) has no current plans to provide transit service in this area. However, the Framework Plan for the FUA identifies SR-56 as a “Transit/HOV (high-occupancy vehicle)” emphasis facility with right-of-way reserved for HOV and possible future transit use. Residents of Del Mar Mesa could access potential future transit services through park-and-ride lots planned in Carmel Valley, Pacific Highlands Ranch and Black Mountain Ranch.

13. Bicycle Circulation

A six-foot-wide Class II bikeway is proposed along both sides of Carmel Mountain Road and Camino Santa Fe in accordance with the Street Design Manual. Bicycling opportunities would be also available along the remaining roadways of Del Mar Mesa along the ten-foot wide multiuse trail system. Typical cross sections for the Class II bikeway and the multiuse trail are illustrated on **Figures 21** and **19**. The bikeway system for the community is depicted on **Figure 22**.

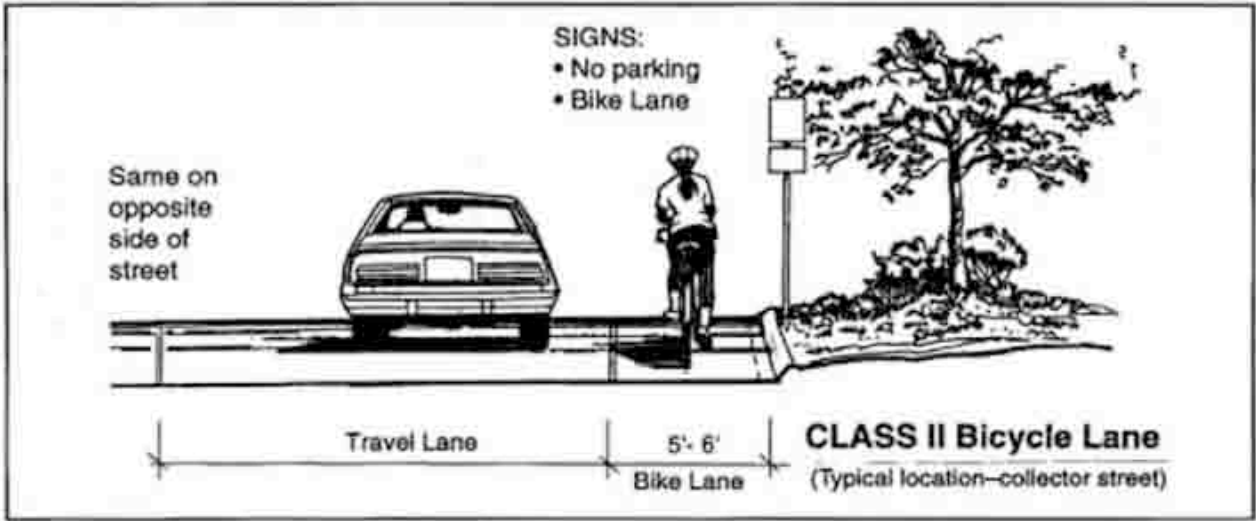
14. Park-and-Ride

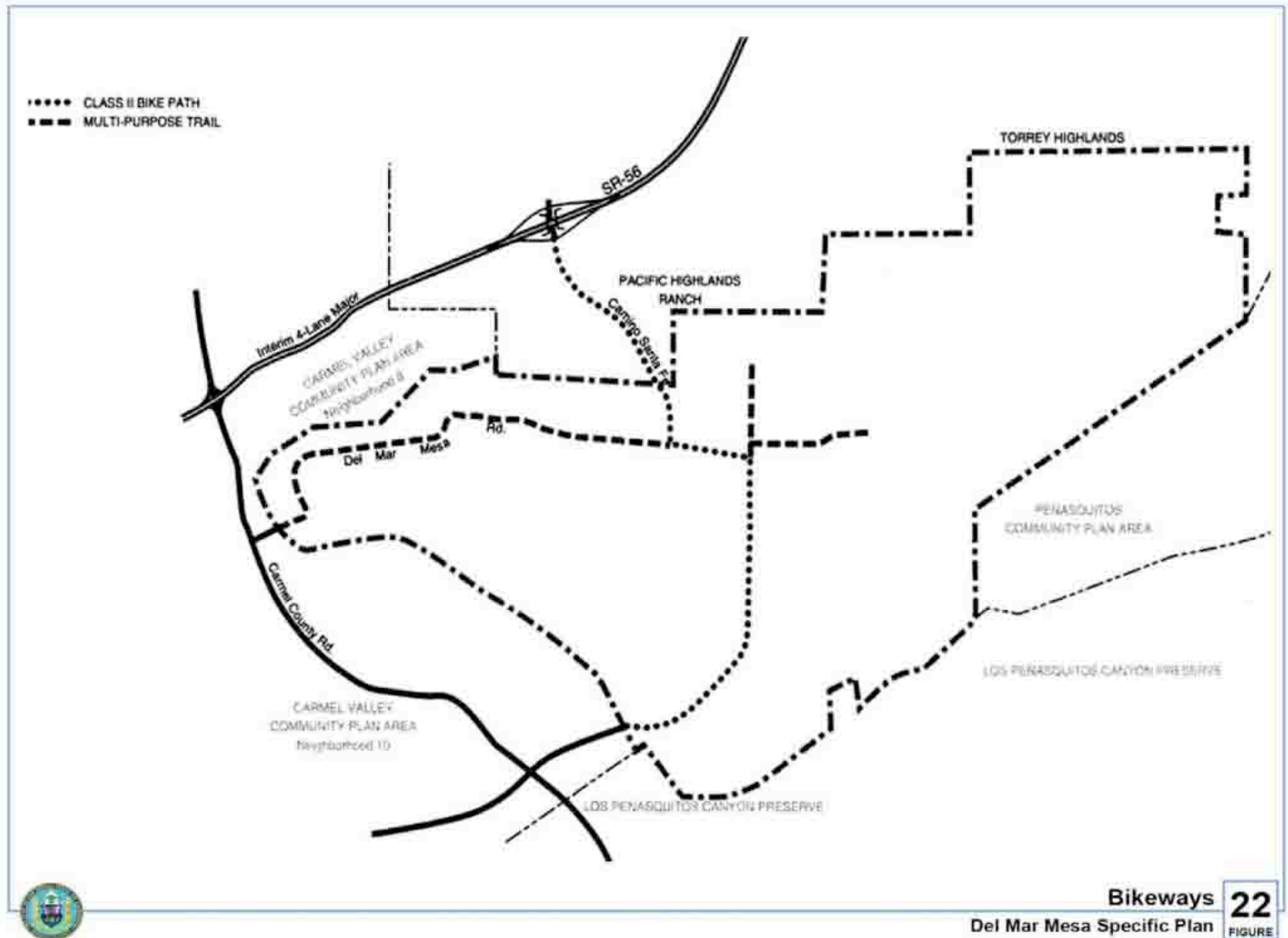
The California Department of Transportation (Caltrans) has a park-and-ride facility south of Carmel Valley Road and west of the I-5/SR-56 interchange. This facility has 68 spaces for commuter parking which are not fully utilized. Caltrans has identified the need for one or two park-and-ride sites as part of the development process for the middle segment of SR-56. The Pacific Highlands Ranch Subarea Plan designates a park-and-ride facility to be located within the employment center to facilitate ride sharing. A second park-and-ride facility is designated in Black Mountain Ranch within the north village.

Both Black Mountain Ranch and Pacific Highlands Ranch are planned to contain transit centers which provide shelter, bike storage and vehicle parking near the mixed-use centers of the communities.

15. Parking

Required parking facilities will be provided by the developers for their respective developments in accordance with zoning requirements.





16. Street Lights

One of the defining characteristics of Del Mar Mesa is the dark night sky. This is a unique resource in a city the size of San Diego and should be preserved. In order to preserve the rural nature of the community and protect the resources in the MHPA, this resource should be maintained in Del Mar Mesa. The Multiple Species Conservation Plan Subarea Plan states:

Lighting shall be designed to avoid intrusion into the MHPA and effects on wildlife. Lighting in areas of wildlife crossings should be of low-sodium or similar design.

In order to preserve this important resource, standard street lighting is not desirable. The only streetlights that should be provided in the community are those necessary for safety as determined by the City Engineer, such as but not limited to, intersections and sharp turns. The street lights that are required for safety shall be efficient, avoiding spill over and lighting only necessary areas. Spillover shall be eliminated through the selection of the appropriate light standard and lighting device (bulb). The standard lighting, where necessary for safety shall be full cut-off, low-pressure sodium lights, mounted on poles with a maximum height of 16 feet. In addition to reduced street lighting, the lighting of private recreational facilities, such as tennis courts, shall not be permitted. Swimming pools may be lit for safety purposes, using ground lighting that does not project more than six feet from the lighting source. The outdoor lighting of single-family residences is permitted and may be installed for the purposes of safety and security. However, this lighting shall minimize the emission of light rays into the night sky and neighboring open spaces and properties. Carefully designed lighting plans are required when submitting for building permits in order to determine the best balance between safety, security and the preservation of the dark night sky and protection of the resources in the MHPA. This lighting plan shall indicate the locations, type and materials of all project lighting and depict the range of this lighting. Lighting is also to be provided per the following MSCP guidelines:

Artificial lighting is generally not a compatible use in preserve areas as it can be detrimental to wildlife use, particularly to nocturnal species. Artificial lighting is not to be provided in preserve areas. Streetlights are to be installed if essential for roadway, facility use and safety. Low voltage outdoor or trail lights, spotlights, or bug lights are prohibited in the preserve.

17. Off-Road Vehicles

Off-road vehicle activity is an incompatible use in the open space area, except by public agencies for maintenance, management, or emergency purposes. Trail and utility access points should include barriers to preclude off-road use.

E. COMMUNITY DESIGN GUIDELINES

While land use and lot sizes have a considerable impact on the rural characteristics of a community, the visual aspects of the community and the design details of the built form are also important components in preserving the rural atmosphere. In the development of the Plan, smaller lot sizes were permitted in order to preserve large tracts of contiguous open space. Although the lot sizes in the Plan area are in some cases smaller than traditional estate lot sizes, the preservation of the rural character is still important and can be achieved through design standards. The development of mass produced tract homes would be inappropriate and contrary to the historical trend of development in the community and to the goal of maintaining and enhancing the rural character of the community. The following design guidelines are intended to preserve the rural character of Del Mar Mesa and encourage private developers to design projects that are compatible with the image and scale of a rural community.

GOAL: DEVELOP THE COMMUNITY OF DEL MAR MESA AS A RURAL COMMUNITY THAT EMPHASIZES OPEN SPACES, DARK NIGHT SKIES, HIKING AND EQUESTRIAN TRAILS AND SENSITIVELY DESIGNED DEVELOPMENTS WHICH COMPLEMENT THE EXISTING TOPOGRAPHY

The Framework Plan contains implementing principles for very low-density and Estate Residential neighborhoods. The following are several of the implementing principles that apply to Del Mar Mesa:

- Lot configuration and site design should emphasize canyons, hillsides and ridges as the visual focus points of neighborhoods. The layout of lots in these neighborhoods should adapt to existing topography and natural features, avoiding standard repetitive lot sizes and shapes.
- Develop clear pedestrian and open space linkages within and between neighborhoods.
- Streets, drives, parking and emergency vehicle access should be aligned to conform, as closely as possible, to existing grades and minimize the need for the grading of slopes. Streets and other built improvements should not greatly alter the physical and visual character of the hillside.

The following principles are those implementing principles which are specific to Del Mar Mesa and refine the Framework Plan principles listed above. These principles should be applied to all development projects in order to preserve the rural character of the community and develop a community consistent with this goal:

- Preserve the rural character of the community by reducing the bulk and scale of structures and integrating subdivisions into the existing topography and vegetation through the use of sensitive design and grading techniques.

- Complement the topography of Del Mar Mesa by designing structures which reflect and complement into the surrounding terrain.
- Utilize landscaping, fencing, street design and sensitive light treatments to preserve the qualities that contribute to the rural character of the community, such as the dark night skies and the open spaces.
- Preserve and enhance the recreational opportunities and promote non-motorized linkages within Del Mar Mesa and the surrounding communities by providing a system of hiking and equestrian trails in the community.

The following design guidelines and regulations which are included in this section are designed to implement these principles and to preserve the rural character of the community:

- Minimum Lot Size: .5 acres unless a specific lot size is specified in the Plan.
- Minimum Street Frontage: The requirement for minimum street frontage can be deviated from in order to preserve finger canyons and other topographic features.
- All residential structures shall be sited to take advantage of views and designed to blend in with the topography. The lot layout shall take advantage of the larger lots to provide a variety of orientations for the homes. The orientation of each lot shall be based upon view potential, topography, climate and exposure and relation to other homes and streets.
- Lot configuration and site design should emphasize canyons, hillsides and ridges as the visual focal points of the neighborhood. The layout of the lots and streets shall adapt to the existing topography and natural features, avoiding standard lot sizes and shapes and minimizing cut and fill.
- In order to reduce the bulk and scale of the homes, architecture should be designed to complement the surrounding vegetation and topography, taking cues from the natural features of the site rather than overwhelming and dominating these features. This may be achieved by dividing the building heights into one and two story components, varying the rooflines and wall planes, providing openings, projections, recesses and other building details. Additionally, entries, arcades, stairs, overhangs and unique, creative building shapes and angles can help to complement the surrounding vegetation and topography and create and define outdoor space. All accessory buildings and garages shall be integrated into the main home as subordinate elements which are designed with the same materials and of the same architectural style.
- The elements described above, such as varying wall planes, projections, recesses and building details, shall be provided on all elevations of each home.

- The location of the garage shall be subordinate to the main home. By reducing the visual impact of the garage, homes may be able to avoid the standard tract appearance and reduce the bulk and scale of the structures. The large lots provide the opportunity to place the garages to the rear of the homes, detached from the homes and oriented away from the street or designed to be side-loaded. Only 50 percent of the square footage for garages oriented away from and placed to the rear of the homes shall be counted towards the maximum lot coverages for each lot. All lots with over .5 acre of flat, graded pad area shall utilize alternative garage orientations.
- Driveways:
 - The number of driveways accessing public streets shall be kept to a minimum. However, U-shaped driveways could be accommodated that have two access points to the public street.
 - The appropriate use of shared driveways is encouraged. Where lots will access a public street, shared driveways shall be used where appropriate to minimize the number of access points to adjacent roadways (see **Figure 23**).
 - The maximum number of units served by a shared driveway shall be four.
 - Minimum shared driveway width: 16 feet with two-foot graded and stoned shoulders on both sides.
 - Paving shall be required in areas where driveway grade is in excess of six percent.
 - Maximum length of shared driveway: 1,000 feet.
 - All driveways in excess of 500 feet shall provide a turnout approved by the Fire Department.
 - Improvement of driveways with asphaltic concrete is not required. If the above improvement is not proposed, four inches of decomposed granite or suitable alternate material may be approved by the City Engineer in lieu of more durable paving on residential driveways. Shared driveways shall conform to all other driveway standards for a single driveway other than property line location.
 - PDPs that include shared driveways shall include a condition requiring a driveway maintenance agreement.
- Landscaping:
 - Street trees should be clustered and set back various distances from the roadways, where possible, to avoid uniform design and permit a minimum six to eight foot root zone for optimal growth of large trees. The multiuse trail can meander through the public right-of-way and a six to ten foot landscape easement can be used to achieve this goal. Suggested street tree species include Pepper, Oak and Sycamore.

- Where possible, native vegetation should be maintained. Landscape design should seek to incorporate the color palette of surrounding native vegetation. Manufactured slopes should be replanted with fire retardant native species, where possible, to control erosion.
- Fencing: The open spaces and spaciousness of the existing community contributes significantly to the rural character of Del Mar Mesa. In order to preserve this feeling of spaciousness and to protect views both to and from the open spaces that surround the development area, solid walls or fences shall not be permitted in front or street side yards except when these fences are necessary for safety reasons. Residential developments may, if fencing is necessary for safety and privacy, have open fencing in the front and street side yards of their lots. The recommended fencing design and material is four-foot post and rail. In addition to post and rail, the following is a list of other acceptable fence materials: native stone, masonry, detailed wrought iron, wood, brick. Fence heights shall not exceed four feet unless specified in the discretionary approval for the project. In the case of side yard fencing that is necessary to screen a backyard pool, a six-foot fence may be constructed of either solid, chain link or wrought iron material. All fences in the community, other than the four-foot high post and rail fence, shall be required to have landscaping that will soften and screen the full height of the fence, either in the form of non-invasive vines, trees or tall shrubs.
- Gated access: Gates shall not be permitted where their placement would preclude access to open space and trails. Gated projects shall be consistent with Council Policy 600-42, Council Policy on Gated Communities.
- Brush Management: Brush Management in Del Mar Mesa shall be consistent with citywide regulations, except in cases where more specific brush management measures are defined in the site specific development regulations located in this plan.
- Signage: For residential developments, signage will be limited to access, litter control and educational purposes. A Comprehensive Sign Plan shall be prepared for all projects proposing signage in excess of access, traffic control, litter control and educational purposes.
- Grading Design Policies: The philosophy behind the Plan was to preserve the steep slopes and designate the flatter portions of the mesa for development. Although the areas designated for development are primarily flat, some canyons were included in the development area, particularly along the edges of the preserve area. Special care shall be taken to preserve these canyons.
- Where grading is necessary, daylight grading at the edges of the preserve is preferred. If grading is proposed adjacent to the preserve, all disturbance including cut and fill slopes, must occur wholly within the development area. Graded areas adjacent to the preserve shall be re-vegetated with native plant species.

- Grading in the community shall be minimized. Grading shall be permitted for only those areas necessary for the construction of streets and homes. Large quantities and areas of grading shall not be permitted as this type of grading is unnecessary for the development of single-family homes and is out of character in a rural community. Where grading is necessary for the placement of streets, homes and accessory uses, landform grading techniques shall be utilized in order to create only new slopes that topographically resemble the natural landforms of the surrounding area. The use of split pads is encouraged in order to accommodate accessory uses such as pools and tennis courts.
- In order to minimize the grading necessary for development, lot configuration and site design shall adapt to the existing topography and complement the natural features of the site. The created lots shall emphasize canyons, hillsides and ridges as visual focal points and avoid standard lot sizes and shapes. Existing topography and landforms, drainage courses, rock outcroppings, vegetation and views shall be incorporated into the design of lots to the maximum extent feasible.

The above grading policies shall apply to all projects proposed in Del Mar Mesa unless alternative grading policies have been established in the site-specific development regulations described in this Plan.

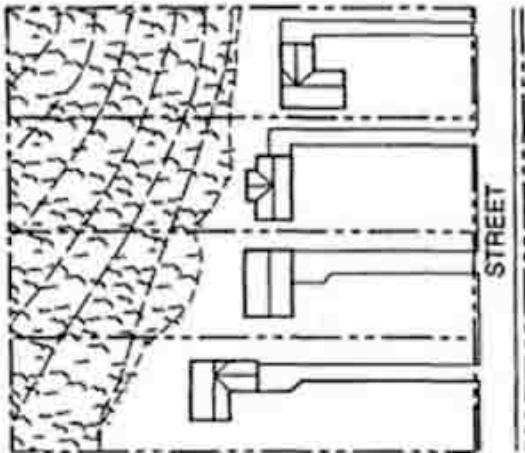
- Planned Development Permit

The San Diego Municipal Code allows deviations from the standard requirements with the use of a planned development permit. Although such deviations are permitted in the Del Mar Mesa community, certain deviations, such as maintaining the minimum lot size and setbacks, would negatively impact the rural character of the community.

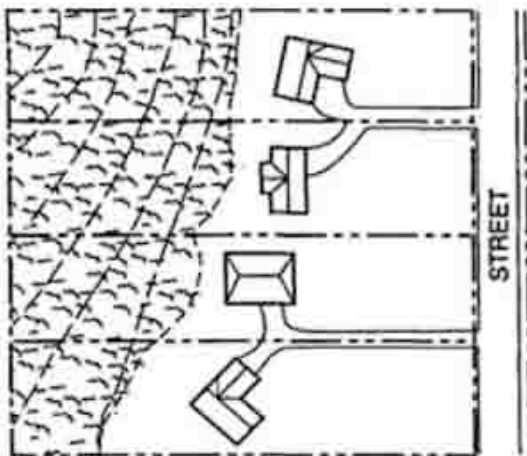
- The minimum setbacks in the Del Mar Mesa community shall be:

| | |
|--------|---|
| Front: | 25 feet |
| Side: | 15 feet (or 30 feet between structures) |
| Rear: | 25 feet |
- The maximum lot coverage for a two-story home shall be 30 percent. The maximum lot coverage for a one-story home shall be 40 percent.
- The minimum lot size shall be 0.50 acre.

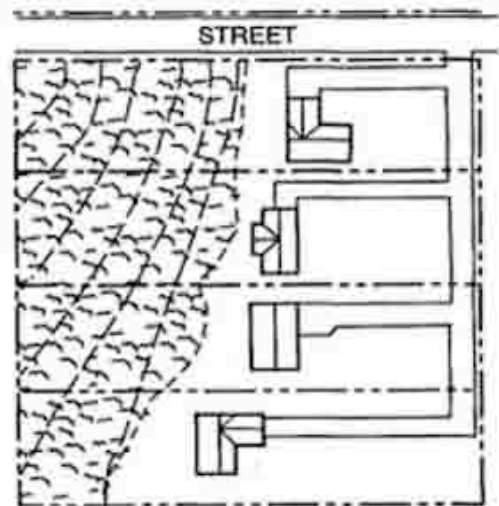
Deviations from these standards will not be permitted unless deemed appropriate for the rural character by the Planning and Development Services departments, or unless otherwise specified in the site-specific development regulations of this Plan. Consideration for deviations should be based on compatibility with surrounding topography and vegetation, surrounding lot sizes and configurations, the architecture proposed for the project and whether the requested deviation results in a project that is superior to a project that could be realized without the requested deviation.



Conventional development of frontage lots with individual drives.



Use of common drives for frontage lots is encouraged.



F. COASTAL ELEMENT

Portions of Del Mar Mesa are located within the Coastal Zone and are governed by the North City Local Coastal Program (LCP), adopted by the City Council and certified by the California Coastal Commission. These include areas designated Estate Residential and Resource Based Open Space in the northwest corner of the Del Mar Mesa and open space areas primarily in public ownership in the southern part of the subarea (see **Figure 5**).

The Plan, in addition to the NCFUA Framework Plan, constitutes the land use plan segment for Del Mar Mesa within the City's LCP. This Plan is intended to implement the Framework Plan and the North City LCP.

The Plan and Plan amendments and ordinances necessary to implement the Plan require certification by the California Coastal Commission in order to become effective in the Coastal Zone areas. The California Coastal Commission certified the Del Mar Mesa Specific Plan on August 13, 1997. Upon this certification, the City assumed coastal permit authority for all areas located in the Coastal Zone within the Del Mar Mesa community.